



110 N. Poplar Street • PO Box 218 • West Branch, Iowa 52358
(319) 643-5888 • Fax (319) 643-2305 • www.westbranchiowa.org • city@westbranchiowa.org

PLANNING AND ZONING COMMISSION MEETING

Tuesday, May 25, 2021 • 7:00 p.m.

****West Branch City Council Chambers, 110 N. Poplar St.**

Council Quorum May Be Present

1. Call to Order
2. Roll Call
3. Approve Agenda/Move to action.
 - a. Approve minutes from the March 23, 2021 Planning & Zoning Commission Meeting. / Move to action.
4. Public Hearing/Non-Consent Agenda. /Move to action.
 - a. Review & discuss West Branch Village Expansion Site Plan.
 - b. Review draft Ordinance 781 – Annexed Territory
 - c. Approved proposed language for Ordinance 781. Move to action.
 - d. Discuss Sign requirements for the HCI District.
 - e. Discuss Comprehensive Plan – Chapter 9 - Transportation
 - f. Review Future Land Use Map
5. Old Business
 - a. Update on Rezoning former Croell property to PUD
6. City Staff Reports
7. Comments from Chair and Commission Members
8. Next regular Planning & Zoning Commission meeting Tuesday, July 27, 2021.
9. Adjourn

In-person participation is encouraged, however not required.

***** Electronic Meeting (Pursuant to Iowa Code section 21.8) An electronic meeting will be available for those not able to attend in person. You can watch and/or participate in the meeting at the following link <https://zoom.us/j/829677991> or dial in phone number 1-312-626-6799 with Meeting ID 829 677 991.***

Planning & Zoning Commission Members: Chair John Fuller, Vice Chair Ryan Bowers, Sally Peck, Emilie Walsh, Brad Bower, Matt Van Scoyoc, Jim Hoffman • **Zoning Administrator:** Terry Goerd • **Deputy Clerk:** Leslie Brick
Mayor: Roger Laughlin • **Council Members:** Colton Miller, Nick Goodweiler, Jodee Stoolman, Jerry Sexton, Tom Dean
City Administrator/Clerk: Redmond Jones II • **Fire Chief:** Kevin Stoolman • **Police Chief:** John Hanna
• **Public Works Director:** Matt Goodale

(These minutes are not approved until the next Commission meeting.)

City of West Branch Planning & Zoning Commission Meeting
March 23, 2021
West Branch City Council Chambers, 110 North Poplar Street

An Electronic Meeting (pursuant to Iowa Code Section 21.8) of the Planning & Zoning Commission of the City of West Branch, Cedar County, IA was held on Tuesday, March 23, 2021 at 7:00 p.m. because a meeting in person was impossible or impractical due to concerns for the health and safety of Commission Members, Staff and the Public presented by COVID-19.

Until further notice, all of our Planning & Zoning Meetings will be held electronically. Persons may still attend, observe and participate in the meeting at the Council Chambers, City Office, 110 N. Poplar St, West Branch, Iowa. Social distancing practices shall be observed for any persons attending the meeting in person at City Hall.

Chairperson John Fuller called the Planning & Zoning Commission meeting to order at 7:00 p.m. Roll call: Commission members present: John Fuller and Ryan Bowers. Via Zoom: Jim Hoffman, Matt Van Scoyoc and Brad Bower. Emilie Walsh and Sally Peck were absent. City Staff present: City Administrator Redmond Jones II, Deputy Clerk Leslie Brick, Terry Goerd, Mayor Roger Laughlin and Councilperson Jerry Sexton.

APPROVE AGENDA/CONSENT AGENDA/MOVE TO ACTION.

Approve the agenda for the March 23, 2021 Planning & Zoning Commission meeting. /Move to action.

Motion by Bowers, second by Fuller to approve the agenda. Absent: Walsh and Peck. Motion carried on a voice vote.

Approve minutes from the January 26, 2021 Planning & Zoning Commission meeting. /Move to action.

Motion by Bowers, second by Van Scoyoc to approve the minutes. Absent: Walsh and Peck. Motion carried on a voice vote.

PUBLIC HEARING/NON-CONSENT AGENDA

Review rezoning request for West Branch Roofing, 322 E. College St.

Phil Rushton, owner of West Branch Roofing presented his proposed building plans for his business on a parcel on the south side of East College Street. Rushton said the building he is wanting to construct will not meet the newly established setbacks for the I-2 district in which the property lies. Rushton said the building will provide more indoor storage for his equipment and roofing materials along with office space. He added that he is currently renting the property directly to the south of this property and there may be a possibility of him purchasing it in the future which would help fit the building on the proposed location. The Commission discussed the idea of rezoning the property to a Planned Unit Development (PUD) since that zoning classification is more flexible than the current zoning and with its close proximity to the former Croell property which is also intended to be rezoned to a PUD. Fuller added that a PUD classification allows the City the right to give input on the proposed structure and how it would fit into the future plan of this area.

Laughlin and Sexton felt that several adjacent properties in the area would all benefit from the PUD rather than an I-2 designation since that is one of the unique features of a PUD and suggested that they could reach out to the property owners to present their ideas. Laughlin stated he would bring the subject up with the City Council as well to get their initial thoughts on the matter.

Review Comprehensive Plan and how the City can implement

Fuller opened the discussion of the Comprehensive Plan and asked the other members how they thought they could help the City help implement some of the ideas in the plan. Fuller listed some goals of the plan such as offering trees to homeowners to help build a tree canopy where trees are lacking, informing residents of the Federal Flood Insurance program, and building a community garden. Fuller suggested that maybe the City could form committees to help with some of the initiatives. Laughlin said he was in favor of the ideas, but forming committees and finding volunteers is never as easy as it would seem but didn't dissuade Fuller for pushing on with his ideas. Fuller added that he would like to continue this discussion in future meetings.

OLD BUSINESS

Review approved Ordinance 780

Brick informed the Commission that the City Council performed the third and final reading of Ordinance 780 – Swimming Pools at their March 15th City Council meeting. The Council ultimately approved the ordinance as written, but did request to make temporary pools with a depth of less than twenty four inches be exempt from the ordinance citing it would be difficult to enforce and limiting for kiddie pools often used by residents. The Commission agreed with the Council's decision.

STAFF REPORTS:

Laughlin reported that the Parkside Hills developer was running into storm water run-off issues which they were in the process of addressing with the City Engineer. Laughlin was hopeful the issues would be resolved quickly and that the project would move forward.

COMMENTS FROM CHAIR AND COMMISSION MEMBERS: NONE

Adjourn

Motion by Bowers, second by Hoffman to adjourn the Planning & Zoning Commission meeting. Motion carried on a voice vote. The meeting adjourned at 7:57 p.m.

Submitted by:

Leslie Brick

Deputy City Clerk



Leslie Brick <leslie@westbranchiowa.org>

West Branch Village Improvement Plans

1 message

Joseph Mailander <jmailander@clearwelleng.com>

Fri, May 7, 2021 at 8:06 AM

To: Matt Goodale <mattg@westbranchiowa.org>

Cc: Redmond Jones <rjonesii@westbranchiowa.org>, Dave Schechinger <dschechinger@v-k.net>, Leslie Brick <leslie@westbranchiowa.org>, Tim Moss <timm@westbranchiowa.org>, Matt Goodale <mgoodale@westbranchiowa.org>, "Dirt Boys Inc. Dirt Boys Inc." <dirtboyssa@yahoo.com>

Good morning Matt,

Attached are engineered improvement plans for the proposed expansion of the existing West Branch Village manufactured home community. The proposed expansion is located in the southwest corner of the property, adjacent to the existing development. I located plans originally drafted by Shive Hattery engineering in 1997, and the current expansion is nearly an exact match to the previous expansion concept.

As we discussed and is shown on the attached plans, the current owners plan to connect to the City's sanitary sewer system. The connection is proposed at the City's last manhole, located south of the lagoons. Water is supplied from the existing connection along 4th Street. A storm water basin is proposed south of the existing lagoons and onsite runoff is routed through this basin.

Please let me know the best way to start the review and discussion for this proposed project. I have copied others on this email, including the general contractor for the project, but if there are others you would like me to include please let me know. I appreciate your time and consideration of this project. Feel free to call or email with any questions.

Sincerely,

Joseph W. Mailander, PE

Civil Engineer / Clearwell, PLLC

CLEARWELL

319-654-4911

jmailander@clearwelleng.com

www.clearwelleng.com

2 attachments



2021-05-07 WBV Building-Permit-Application.pdf
266K



2021-05-06 WEST BRANCH VILLAGE IMP PLANS.pdf
2137K

WEST BRANCH VILLAGE EXPANSION PLANS
WEST BRANCH, IOWA

UTILITY COMPANY CONTACTS	
IOWA ONE-CALL	811
LINN COUNTY REC	319-377-1587
ALLIANT ENERGY	800-255-4268
WATER - WEST BRANCH	319-643-5888
SEWER - WEST BRANCH	319-643-5888
LIBERTY COMMUNICATIONS	319-643-5353
CENTURY LINK	800-289-1901
MEDIACOM IOWA CITY	800-332-0245

CITY OF WEST BRANCH CONTACT INFORMATION	
CITY ADMINISTRATOR	319-643-5888
POLICE DEPARTMENT	563-886-2121
FIRE DEPARTMENT	319-643-2110
PUBLIC WORKS DEPARTMENT	319-643-5888
BUILDING & INSPECTIONS	319-330-9806

SURVEY BENCHMARKS AND CONTROL

SURVEY COMPLETED BY XCEL CONSULTANTS, INC OF ROCK ISLAND, ILLINOIS
PH: 309-787-9988

FOUND MONUMENT, PK NAIL
N: 621355.8520
E: 2228584.1460
ELEV: 727.131

FOUND MONUMENT, PINK CAP IR, PLS HEALY 23886
N: 621335.4990
E: 2229848.9980
ELEV: 773.640

FOUND MONUMENT, PINK CAP IR, PLS HEALY 23886
N: 618915.9780
E: 2229891.5890
ELEV: 732.096

CONTROL POINT #1
SET IR
N: 619782.9160
E: 2229279.5050
ELEV: 732.570

CONTROL POINT #3
SET IR
N: 619048.8720
E: 2229568.9910
ELEV: 750.657



KEY MAP
1" = 200'

APPLICABLE STANDARDS

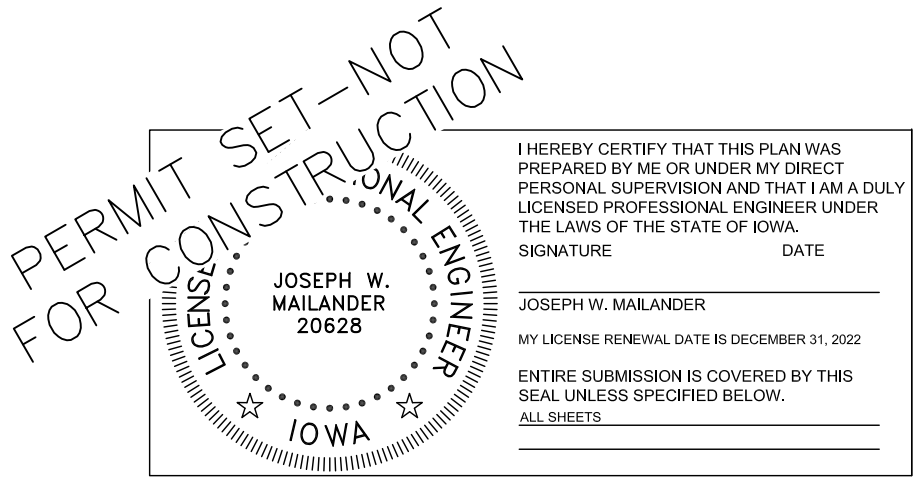
- IOWA STANDARD URBAN DESIGN STANDARDS (SUDAS), 2020 EDITION
- ALL WORK SHALL CONFORM TO CITY OF WEST BRANCH SUPPLEMENTAL SPECIFICATIONS
- ALL APPLICABLE PIPE MANUFACTURER JOINING PROCEDURES

GENERAL NOTES

- THE UTILITY LOCATIONS SHOWN ARE AS MARKED BY THEIR OWNERS, FROM THE OWNERS RECORDS, OF FROM FIELD EVIDENCE. THE SIZE AND LOCATION OF THE UNDERGROUND UTILITIES WAS NOT VERIFIED BY EXCAVATION. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING AND PROTECTING UTILITIES DURING CONSTRUCTION.

INDEX OF SHEETS	
A.01	TITLE SHEET
A.02	GENERAL NOTES & QUANTITIES
B.01	DETAILS
C.01-C.03	SITE PLAN, DEMOLITION & GRADING, EROSION CONTROL
D.01-D.05	PAVING & STORM SEWER PLANS
S.01-S.04	SANITARY SEWER PLANS
W.01-W.03	WATER MAIN PLANS

EXISTING	DESCRIPTION	PROPOSED
---	RIGHT OF WAY	---
---	PROPERTY LINE	---
---	CONSTRUCTION LIMIT	---
////	BUILDING LINE	////
E	ELECTRICAL	E
FOP	FIBER OPTIC LINE	FOP
G	GAS	G
SAN	SANITARY SEWER	SAN
ST	STORM SEWER	ST
TEL	TELEPHONE	TEL
W	WATERMAIN	W
X	FENCE LINE	X
TV	TELEVISION	TV
[Symbol]	INTAKE	[Symbol]
[Symbol]	SANITARY MANHOLE	[Symbol]
[Symbol]	STORM MANHOLE	[Symbol]
[Symbol]	UTILITY MANHOLE	[Symbol]
[Symbol]	WATER VALVE	[Symbol]
[Symbol]	VALVE VAULT	[Symbol]
[Symbol]	HYDRANT	[Symbol]
[Symbol]	WATER SERVICE BOX	[Symbol]
[Symbol]	GAS VALVE	[Symbol]
[Symbol]	LIGHT FIXTURE	[Symbol]
[Symbol]	STREET LIGHT	[Symbol]
[Symbol]	POWER POLE	[Symbol]
[Symbol]	TELEPHONE PEDESTAL	[Symbol]
[Symbol]	ELECTRIC PEDESTAL	[Symbol]



GENERAL NOTES

- DIMENSIONS, STREET LOCATIONS, UTILITIES AND GRADING ARE BASED ON AVAILABLE INFORMATION AT THE TIME OF DESIGN. DEVIATIONS MAY BE NECESSARY IN THE FIELD. ANY SUCH CHANGES OR CONFLICTS BETWEEN THE PLAN AND FIELD CONDITIONS SHALL BE REPORTED TO THE CITY INSPECTOR.
- CONFINE ALL CONSTRUCTION ACTIVITY TO THE CONSTRUCTION LIMITS UNLESS OTHERWISE AUTHORIZED.
- DO NOT STORE EQUIPMENT AND/OR MATERIALS WITHIN PUBLIC RIGHT-OF-WAY, ON STREETS AND/OR ALLEYS OPEN TO TRAFFIC. CONTRACTOR SHALL PROVIDE AREAS AS NEEDED FOR STORAGE OF EQUIPMENT AND/OR MATERIALS.
- WHERE CONTRACTOR EQUIPMENT IS OPERATED ON ANY PORTION OF THE PAVEMENT OR STRUCTURES USED BY TRAFFIC ON OR ADJACENT TO THE SECTION UNDER CONSTRUCTION, THE CONTRACTOR SHALL CLEAN THE PAVEMENT OF ALL DIRT AND DEBRIS AT THE END OF EACH DAY'S OPERATIONS, AND AT OTHER TIMES AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR IS RESPONSIBLE FOR ALL SITE SAFETY INCLUDING FENCING AND SIGNAGE ON SITE AND SHALL COMPLY WITH ALL STATE, LOCAL AND FEDERAL REGULATIONS.
- THE CONTRACTOR SHALL COMPLY WITH ALL STATE REGULATIONS REGARDING AIR, WATER AND NOISE POLLUTION.
- WHERE SECTION OF SUBSECTION MONUMENTS, BENCHMARKS, RIGHT-OF-WAY PINS, OR IRON PIPE MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED OR DISTURBED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL MONUMENTS UNTIL AN AUTHORIZED SURVEYOR, OR AGENT, HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. THE CONTRACTOR WILL BE RESPONSIBLE FOR HAVING AN AUTHORIZED SURVEYOR RE-ESTABLISH ANY MONUMENTS UNNECESSARILY DESTROYED BY CONTRACT OPERATIONS.
- FULL DEPTH SAW CUT EXISTING PAVEMENTS PRIOR TO REMOVALS. THIS IS INCIDENTAL TO REMOVALS PAY ITEMS.
- PROTECT EXISTING FACILITIES, TREES AND OTHER APPURTENANCES NOT TO BE REMOVED FROM THE SITE DURING CONSTRUCTION.
- COORDINATE CONSTRUCTION TO MINIMIZE DISRUPTIONS TO THE ADJACENT PROPERTIES. REPAIR AND RESTORE ANY AREAS DISTURBED BY CONSTRUCTION OUTSIDE OF THE CONSTRUCTION LIMITS AT THE CONTRACTOR'S EXPENSE.
- DURING CONSTRUCTION, USE ALL MEANS NECESSARY TO CONTROL DUST SPREADING FROM WORK AND STAGING AREAS. DUST CONTROL MEASURES SHALL BE IN ACCORDANCE WITH APPLICABLE SPECIFICATIONS, OR APPROVED BY THE ENGINEER. DUST CONTROL MEASURES ARE CONSIDERED INCIDENTAL.
- CONTRACTOR SHALL VERIFY SANITARY AND STORM SEWER PIPE SIZES AND INVERTS PRIOR TO CONSTRUCTION AND PRIOR TO ORDERING MANHOLES AND/OR PIPE.
- CONTRACTOR SHALL REPAIR ALL FIELD/DRAIN TILES ENCOUNTERED DURING CONSTRUCTION AS SPECIFIED OR AT A MINIMUM TO ALLOW FLOW USING LIKE MATERIAL IN NEW CONDITION WITH COUNTY APPROVED CONNECTIONS. CONTRACTOR SHALL RECORD EXISTING TYPE, SIZE, LOCATION AND DEPTH OF ALL FIELD/DRAIN TILES ENCOUNTERED AND REPAIRED DURING CONSTRUCTION. PROVIDE DATA TO THE CITY FOR INCORPORATION INTO RECORD DRAWINGS.

GENERAL UTILITY NOTES

- REFER TO TABULATION OF UTILITY GRADE LEVELS IN ACCORDANCE WITH C/ASCE 38-02 IN THESE DRAWINGS FOR INFORMATION REGARDING DEPICTION OF EXISTING SUBSURFACE UTILITIES. THE CONTRACTOR MUST VERIFY ACTUAL LOCATIONS IN THE FIELD.
- UTILITY LOCATIONS AND DEPTHS WERE PLOTTED FROM INFORMATION FURNISHED BY THE VARIOUS UTILITY COMPANIES AND THEIR ACCURACY SHOULD BE CONSIDERED APPROXIMATE. NO RESPONSIBILITY IS ACCEPTED FOR THE LOCATIONS AS SHOWN OR THAT ALL UTILITY FACILITIES ARE SHOWN.
- UTILITY LOCATIONS SHOWN IN THE PLANS ARE APPROXIMATE. UNLESS NOTED OTHERWISE, THEY REPRESENT LOCATIONS PRIOR TO ANY UTILITY RELOCATIONS REQUIRED TO ACCOMMODATE PROPOSED CONSTRUCTION. GAS VALVES AND RELATED FIXTURES, TELEPHONE PEDESTALS AND RELATED FIXTURES, UTILITY MANHOLES, POWER POLES, AND STREET LIGHT POLES SHALL BE RELOCATED BY THE UTILITY. THE CONTRACTOR SHALL COOPERATE WITH THE OWNERS OF THESE UTILITIES IN THEIR REMOVAL AND REARRANGEMENT OPERATIONS SO WORK MAY PROGRESS IN A REASONABLE MANNER. DUPLICATION OF REARRANGEMENT WORK MAY BE REDUCED TO A MINIMUM, AND SERVICES RENDERED BY THOSE PARTIES WILL NOT BE UNNECESSARILY INTERRUPTED. WHERE INDICATED IN THE CONTRACT DOCUMENTS, PROVIDE TRAFFIC CONTROL AND COORDINATE SCHEDULING OF UTILITY RELOCATIONS. SCHEDULE UTILITY RELOCATIONS TO AVOID DELAY TO CONTRACTOR OPERATIONS. THE COST FOR LOCATING EXISTING UTILITIES, COORDINATING RELOCATION WORK, PROVIDING TEMPORARY SUPPORTS, AND STAGING CONSTRUCTION TO ACCOMMODATE THE RELOCATION OF UTILITIES IS CONSIDERED INCIDENTAL TO CONSTRUCTION.
- THERE MAY BE ABANDONED UTILITIES WITHIN THE CONSTRUCTION LIMITS NOT SHOWN ON THESE PLANS. THE CONTRACTOR SHALL CONFIRM SAID UTILITIES ARE ABANDONED. THE CONTRACTOR IS RESPONSIBLE FOR REMOVING THESE LINES AS NECESSARY FOR CONSTRUCTION.
- CONTRACTOR SHALL CONFIRM ASSUMED DEPTHS OF UTILITIES AND PROTECT THEM AS NECESSARY DURING CONSTRUCTION.
- CONTRACTOR SHALL VERIFY SANITARY AND STORM SEWER PIPE SIZES AND INVERTS PRIOR TO CONSTRUCTION AND PRIOR TO ORDERING MANHOLES AND/OR PIPE. NO ADDITIONAL PAYMENT SHALL BE MADE TO THE CONTRACTOR FOR MATERIAL THAT IS ORDERED AND DOES NOT MATCH PIPE SIZES AND INVERTS THAT ARE TO BE CONFIRMED PRIOR TO CONSTRUCTION.
- AT POINTS WHERE THE CONTRACTOR'S OPERATIONS ARE ADJACENT TO FACILITIES OF UTILITY COMPANIES, DAMAGE TO WHICH MIGHT RESULT IN CONSIDERABLE EXPENSE, LOSS, OR INCONVENIENCE, WORK SHALL NOT COMMENCE UNTIL ALL ARRANGEMENTS NECESSARY FOR THE PROTECTION THEREOF HAVE BEEN MADE.
- IN THE EVENT OF INTERRUPTION OF UTILITY SERVICES AS A RESULT OF ACCIDENTAL BREAKAGE OR AS A RESULT OF BEING EXPOSED OR UNSUPPORTED, THE CONTRACTOR SHALL PROMPTLY NOTIFY THE PROPER AUTHORITY AND SHALL COOPERATE WITH SAID AUTHORITY IN THE RESTORATION OF SERVICE. NO CLAIMS FOR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR FOR ANY INTERFERENCE OR DELAY CAUSED BY SUCH WORK.
- REPAIR ALL FIELD/DRAIN TILES ENCOUNTERED DURING CONSTRUCTION AS SPECIFIED OR AT A MINIMUM TO ALLOW FLOW USING LIKE MATERIAL IN NEW CONDITION WITH CITY APPROVED CONNECTIONS. THE CONTRACTOR SHALL RECORD EXISTING TYPE, SIZE, LOCATION AND DEPTH OF ALL FIELD/DRAIN TILES ENCOUNTERED AND REPAIRED DURING CONSTRUCTION. PROVIDE DATA TO THE CITY FOR INCORPORATION INTO RECORD DRAWINGS.
- THE CONTRACTOR SHALL OBTAIN APPROVAL FROM THE ENGINEER FOR ANY VARIANCE FROM THE APPROVED PLAN.

SUBSURFACE UTILITY NOTES

THESE DESIGN DRAWINGS INCORPORATE UNDERGROUND UTILITY INFORMATION BASED UPON SUBSURFACE UTILITY ENGINEERING PRACTICES AND STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA (C/ASCE 38-02).

UTILITY QUALITY LEVEL A: PRECISE HORIZONTAL AND VERTICAL LOCATION OF UTILITIES OBTAINED BY THE ACTUAL EXPOSURE (OR VERIFICATION OF PREVIOUSLY EXPOSED AND SURVEYED UTILITIES) AND SUBSEQUENT MEASUREMENT OF SUBSURFACE UTILITIES, USUALLY AT A SPECIFIC POINT. MINIMALLY INTRUSIVE EXCAVATION EQUIPMENT IS TYPICALLY USED TO MINIMIZE THE POTENTIAL FOR UTILITY DAMAGE. A PRECISE HORIZONTAL AND VERTICAL LOCATION, AS WELL AS OTHER UTILITY ATTRIBUTES, IS SHOWN ON PLAN DOCUMENTS. ACCURACY IS TYPICALLY SET TO 15-MM VERTICAL AND TO APPLICABLE HORIZONTAL SURVEY AND MAPPING ACCURACY AS DEFINED OR EXPECTED BY THE PROJECT OWNER.

UTILITY QUALITY LEVEL B: INFORMATION OBTAINED THROUGH THE APPLICATION OF APPROPRIATE SURFACE GEOPHYSICAL METHODS TO DETERMINE THE EXISTENCE AND APPROXIMATE HORIZONTAL POSITION OF SUBSURFACE UTILITIES. QUALITY LEVEL B DATA SHOULD BE REPRODUCIBLE BY SURFACE GEOPHYSICS AT ANY POINT OF THEIR DEPICTION. THIS INFORMATION IS SURVEYED TO APPLICABLE TOLERANCES DEFINED BY THE PROJECT AND REDUCED ONTO PLAN DOCUMENTS.

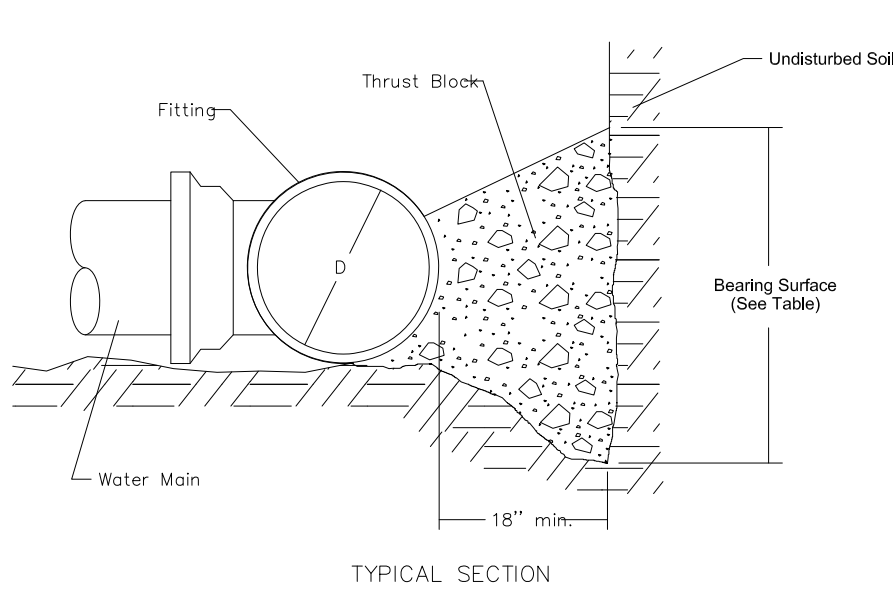
UTILITY QUALITY LEVEL C: INFORMATION OBTAINED BY SURVEYING AND PLOTTING VISIBLE ABOVE-GROUND UTILITY FEATURES AND BY USING PROFESSIONAL JUDGMENT IN CORRELATING THIS INFORMATION TO QUALITY LEVEL D INFORMATION.

UTILITY QUALITY LEVEL D: INFORMATION DERIVED FROM EXISTING RECORDS OR ORAL RECOLLECTIONS.

TABLE OF QUALITY LEVEL OF UTILITY INVESTIGATION IN ACCORDANCE WITH C/ASCE 38-02	
UTILITY	QUALITY LEVEL
TELEPHONE DUCTS (UNDERGROUND)	C
ELECTRIC POLES	C
UNDERGROUND POWER	C
FIBER OPTIC	C
EXISTING SANITARY AND STORM SEWER	C
EXISTING WATER MAIN	C
GAS MAIN	C
CABLE TV	C

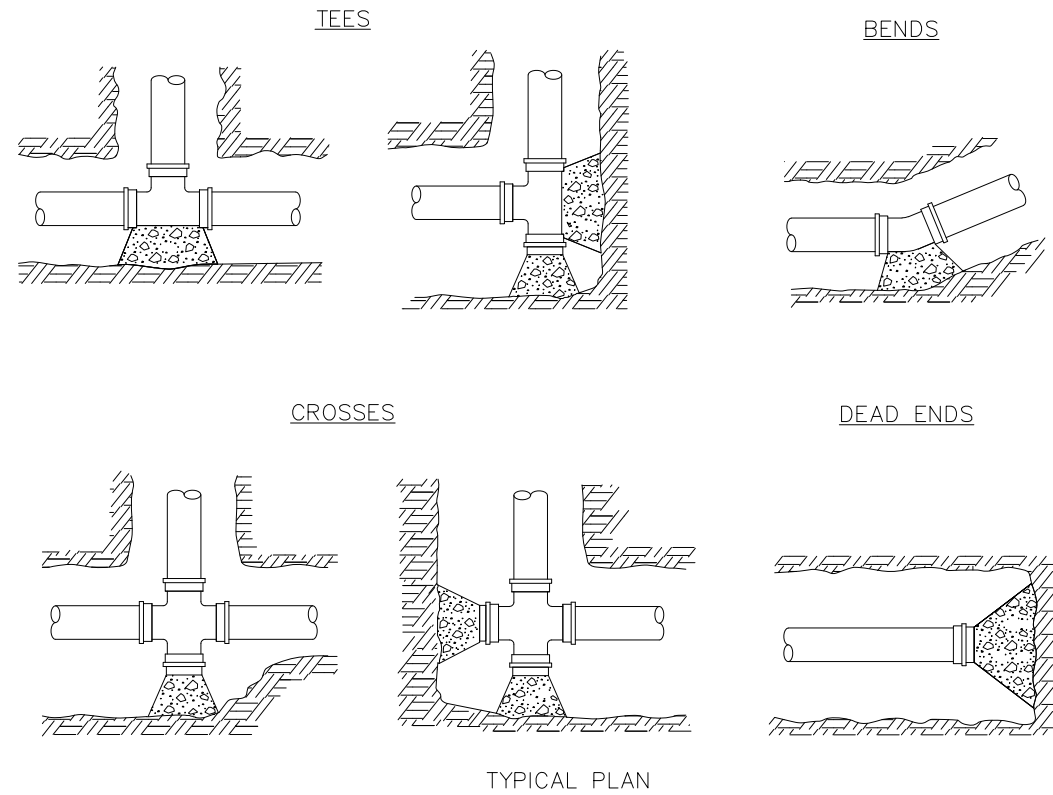
STAGING NOTES

- THE CONTRACTOR SHALL COORDINATE WITH ALL PUBLIC AND PRIVATE PROJECTS IN THE AREA AT ALL TIMES.
- UNDERGROUND FACILITIES INFORMATION REQUIRES VERBAL NOTICE TO ONE CALL NOT LESS THAN 48 HOURS BEFORE EXCAVATING, EXCLUDING WEEKENDS AND LEGAL HOLIDAYS.
- THE UTILITY COMPANIES WHOSE FACILITIES ARE SHOWN ON THE PLANS OR KNOWN TO BE WITHIN CONSTRUCTION LIMITS, SHALL BE NOTIFIED BY THE CONTRACTOR PRIOR TO START OF CONSTRUCTION.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE APPROVED WASTE AREAS OR DISPOSAL SITES FOR EXCESS MATERIAL (EXCAVATED MATERIAL OR BROKEN CONCRETE) WHICH IS NOT DESIRABLE TO BE INCORPORATED INTO THE WORK INVOLVED ON THIS PROJECT
- ALL HOLES RESULTING FROM OPERATIONS OF THE CONTRACTOR INCLUDING REMOVAL OF FENCE POSTS, UTILITY POLES, TREES OR FOUNDATION STRUCTURES SHALL BE FILLED AND CONSOLIDATED TO FINISHED GRADE AS DIRECTED BY THE PROJECT ENGINEER TO PREVENT FUTURE SETTLEMENT. THE VOIDS SHALL BE FILLED AS SOON AS PRACTICAL, PREFERABLY THE DAY CREATED AND NOT LATER THAN THE FOLLOWING DAY. HOLES NEAR PEDESTRIAN WALKWAYS SHALL BE FENCED OFF AT ALL TIMES AND IF PRACTICAL COVERED WITH BOARDS DURING THE OVERNIGHT HOURS AND AT TIMES WHERE WORK IS NOT TAKING PLACE AT THE LOCATION OF THE HOLE FOR AN EXTENDED PERIOD OF TIME. ANY PORTION OF THE RIGHT-OF-WAY OR PROJECT LIMITS (INCLUDING BORROW AREAS AND OPERATION AREAS) DISTURBED BY ANY SUCH OPERATIONS SHALL BE RESTORED TO AN ACCEPTABLE CONDITION. THIS OPERATION SHALL BE CONSIDERED INCIDENTAL TO OTHER BID ITEMS IN THE PROJECT.
- EROSION CONTROL MEASURES NECESSARY TO PROTECT AGAINST SILTATION, EROSION AND DUST POLLUTION ON THE PROJECT SITE SHALL COMPLY WITH SOIL EROSION CONTROL REQUIREMENTS OF IOWA CODE AND LOCAL ORDINANCES.
- THE CONTRACTOR SHALL TAKE SPECIAL CARE TO MAINTAIN POSITIVE DRAINAGE WHERE INTAKES ARE LOCATED.
- THE CONTRACTOR SHALL MAINTAIN ACCESS TO INDIVIDUAL PROPERTIES THROUGHOUT THE CONSTRUCTION OF THE PROJECT. THE CONTRACTOR SHALL NOTIFY PROPERTY OWNERS 48 HOURS IN ADVANCE OF DRIVEWAY MODIFICATIONS THAT WILL TEMPORARILY LIMIT OWNER ACCESS TO THEIR PROPERTY.
- THE CONTRACTOR SHALL CONFINE WORK TO THE CONSTRUCTION LIMITS AND RIGHT-OF-WAY. IF THE CONTRACTOR OBTAINS ADDITIONAL EASEMENTS FOR THE STORAGE OF EQUIPMENT AND MATERIALS, COPIES OF THE AGREEMENTS WITH THE PROPERTY OWNERS SHALL BE PROVIDED TO THE PROJECT ENGINEER.
- THE CONTRACTOR SHALL NOT DISTURB GRASS AREAS OR TREES OUTSIDE THE CONSTRUCTION LIMITS. THE CONTRACTOR WILL NOT BE PERMITTED TO PARK OR SERVICE VEHICLES OR EQUIPMENT OR USE THESE AREAS FOR STORAGE OF MATERIALS. ALL STORAGE, PARKING AND SERVICE AREA(S) WILL BE SUBJECT TO THE APPROVAL OF THE PROJECT ENGINEER.
- DO NOT DRIVE ON THE PAVEMENT WITH CONSTRUCTION TRAFFIC GREATER THAN THE LEGAL LOAD LIMITS.

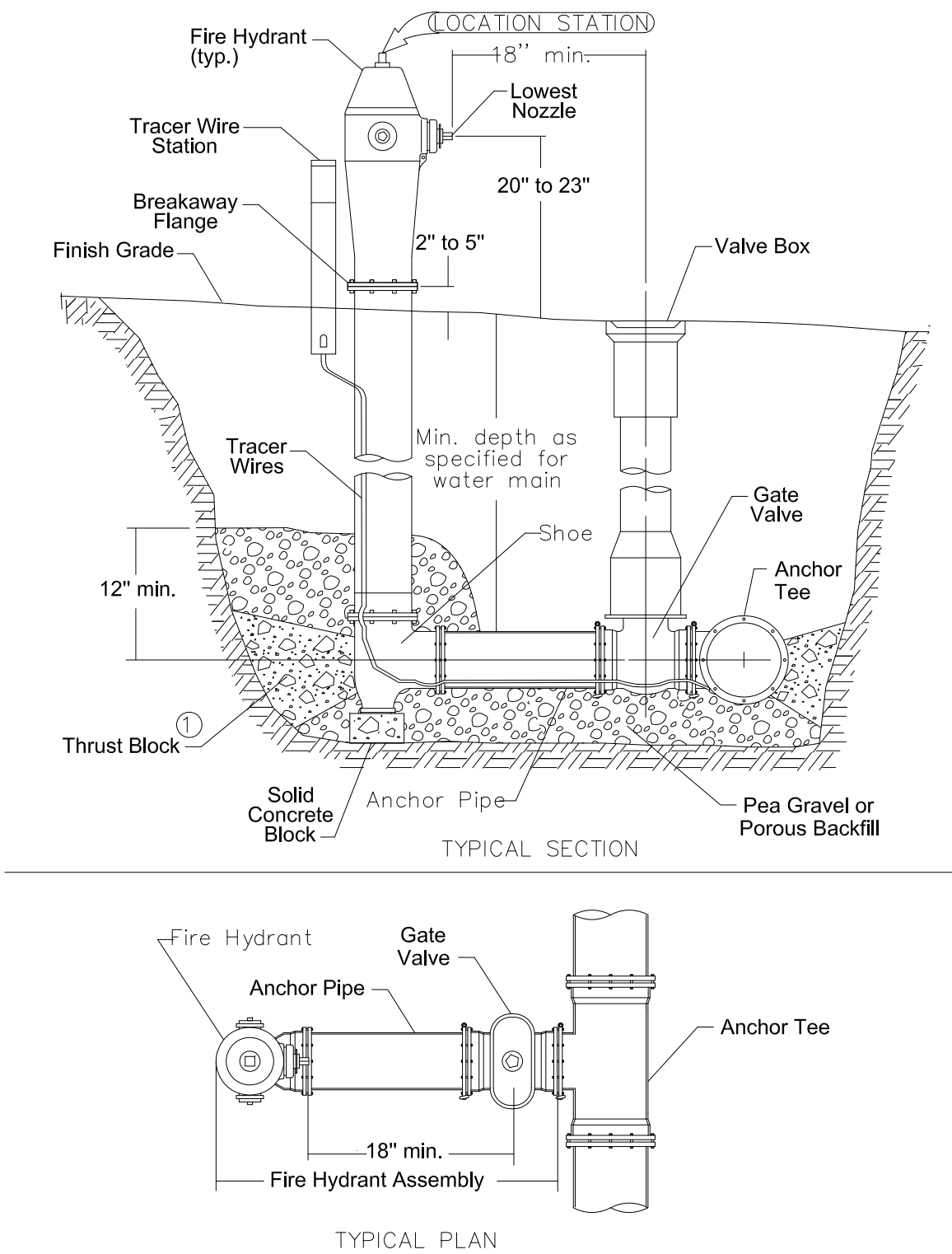


Diameter of Pipe, D (inches)	MINIMUM BEARING SURFACE (sf)				
	11½"	22½"	45"	90"	Tees and Dead Ends
4	1	1	2	4	3
6	1	2	4	8	6
8	2	4	7	14	10
10	3	6	11	21	15
12	4	8	16	29	21
14	5	11	21	39	28
16	7	14	27	50	36
18	9	17	34	63	45
20	11	21	42	78	55
24	15	31	60	111	78
30	24	47	92	171	120
36	34	67	132	244	173

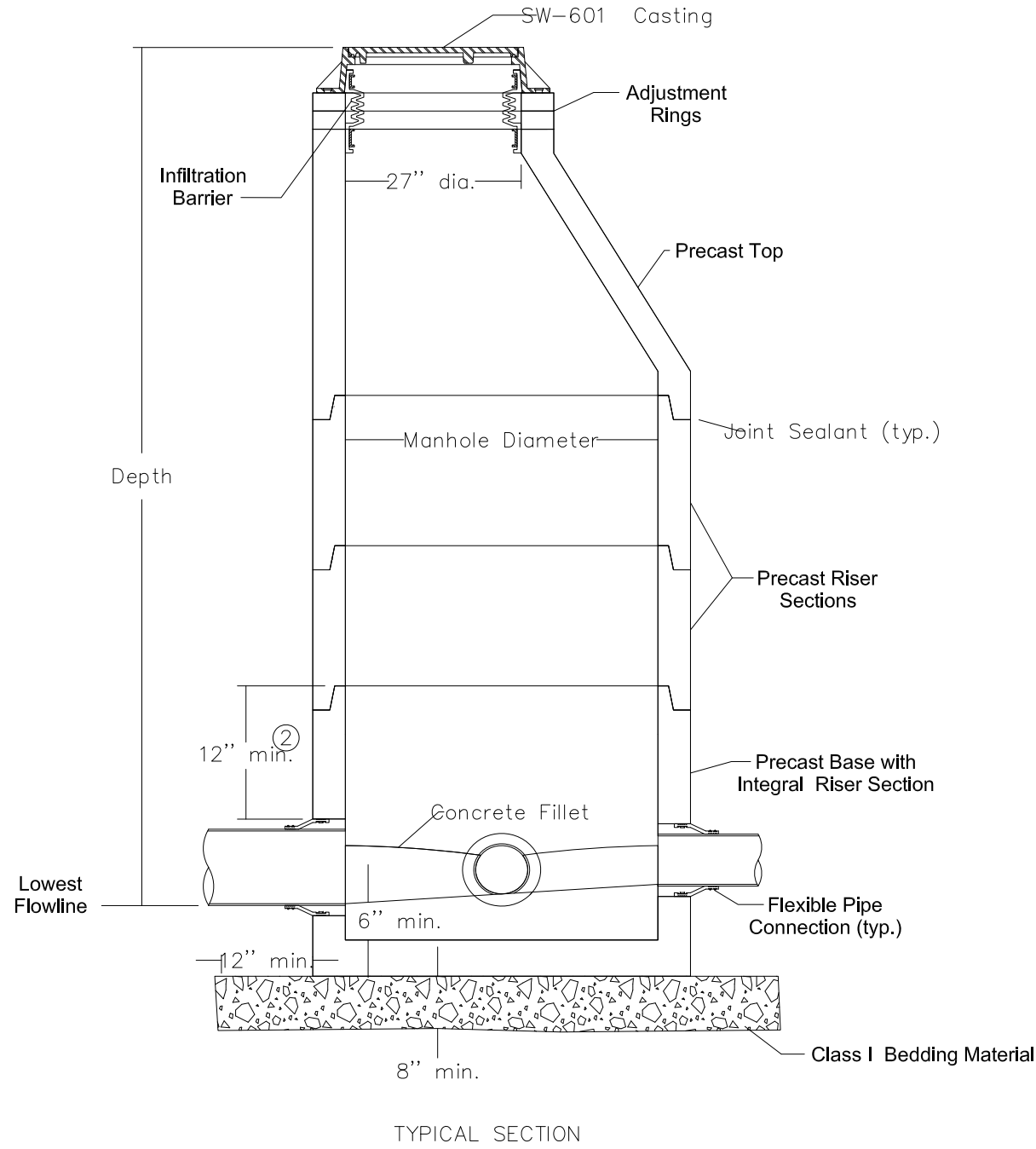
Minimum surface area based on water pressure of 150 psi and allowable soil pressure of 1,000 psf.



WATER THRUST BLOCK DETAILS (WM-101)

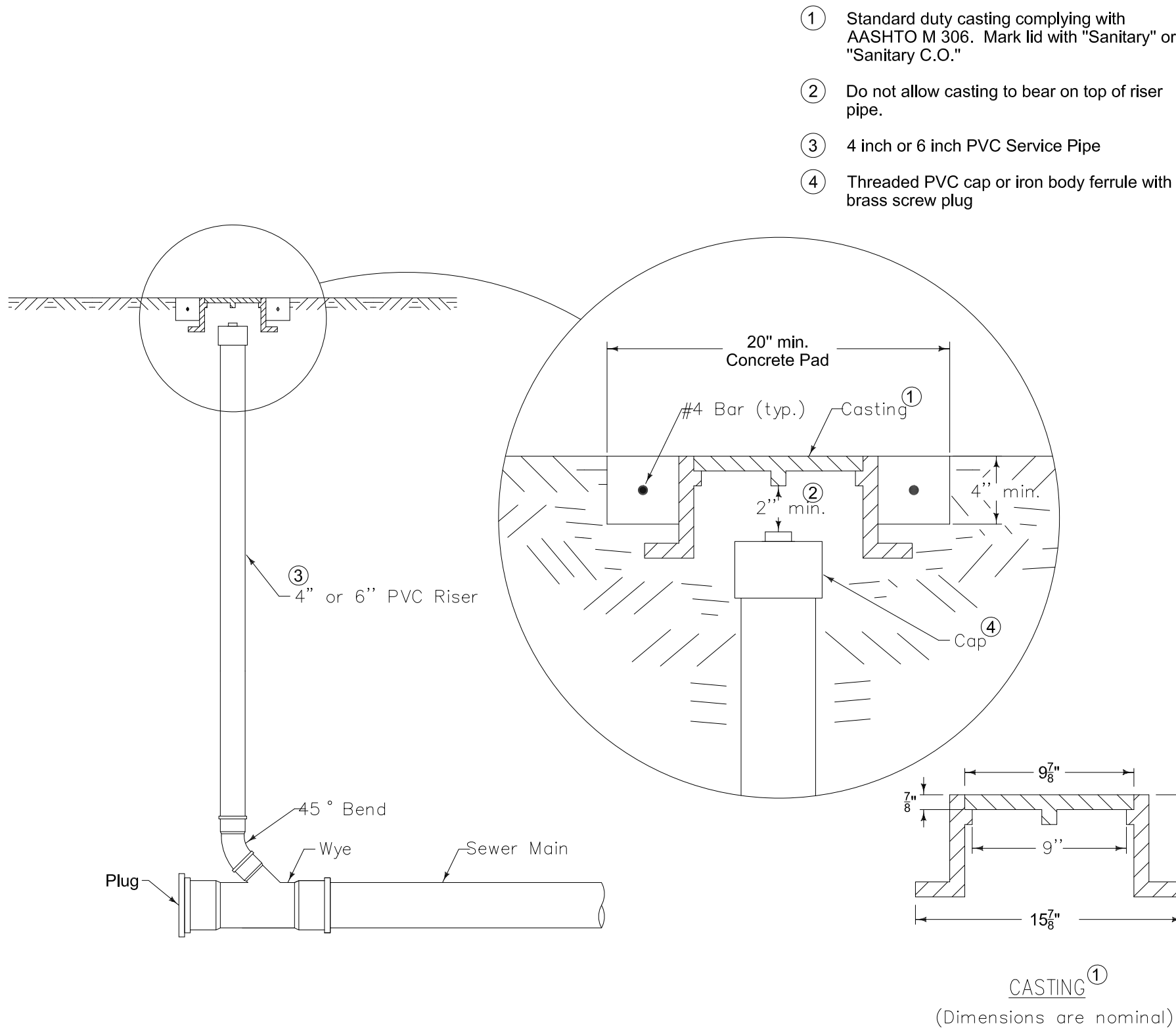
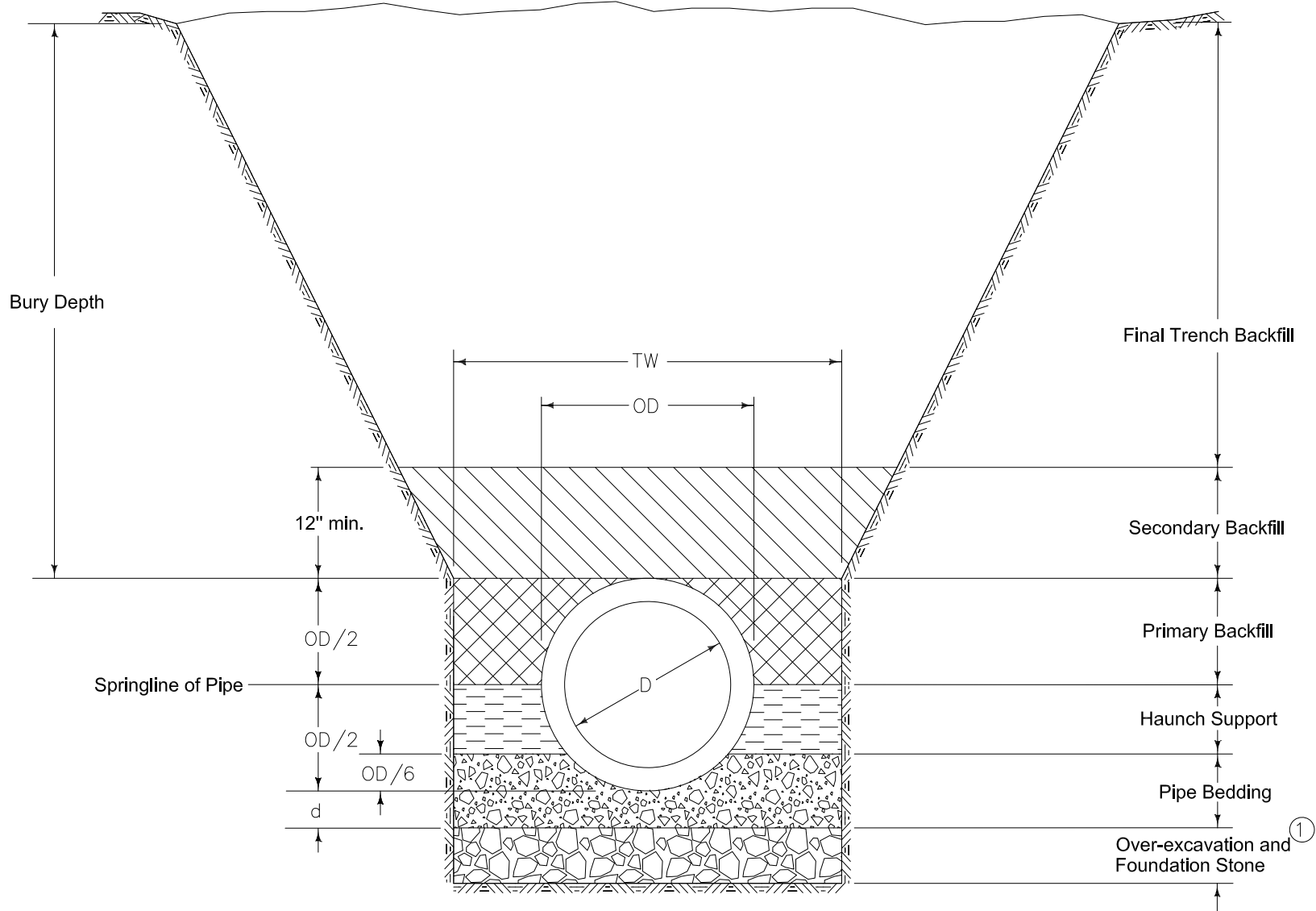


FIRE HYDRANT ASSEMBLY DETAIL (WM-201)



CIRCULAR SANITARY SEWER MANHOLE DETAIL (SW-301)

- Refer to the contract documents for specific material and placement requirements.
- ① Required only when specified in the contract documents or when directed by the Engineer.
- Key**
- OD = Outside diameter of pipe
 - D = Inside diameter of pipe
 - TW = Trench width at top of pipe
 - d = Depth of bedding material below pipe



NOT USED

DRAWN BY: RLC APPROVED BY: JWM
 DATE: May 06, 11:35pm
WEST BRANCH VILLAGE IMP PLANS.dwg | B.01

NO.	REVISION DESCRIPTION	APPROVED	DATE
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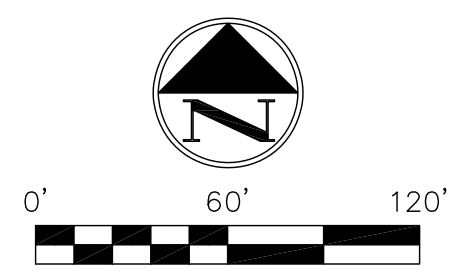
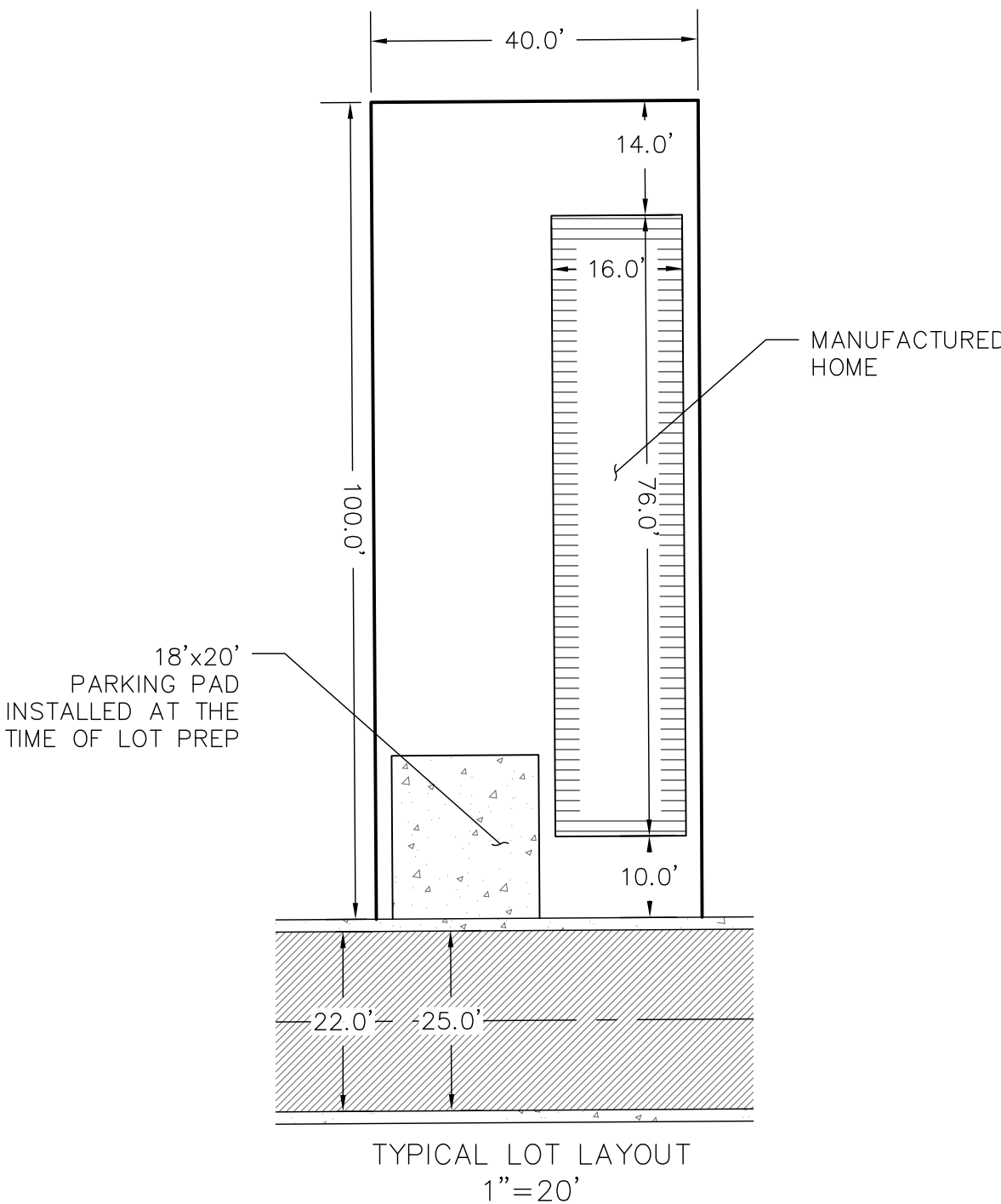
CLEARWELL

2135 BLAKE BLVD SE
 CEDAR RAPIDS, IOWA 52403
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WEST BRANCH, IOWA

DETAILS

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B.01



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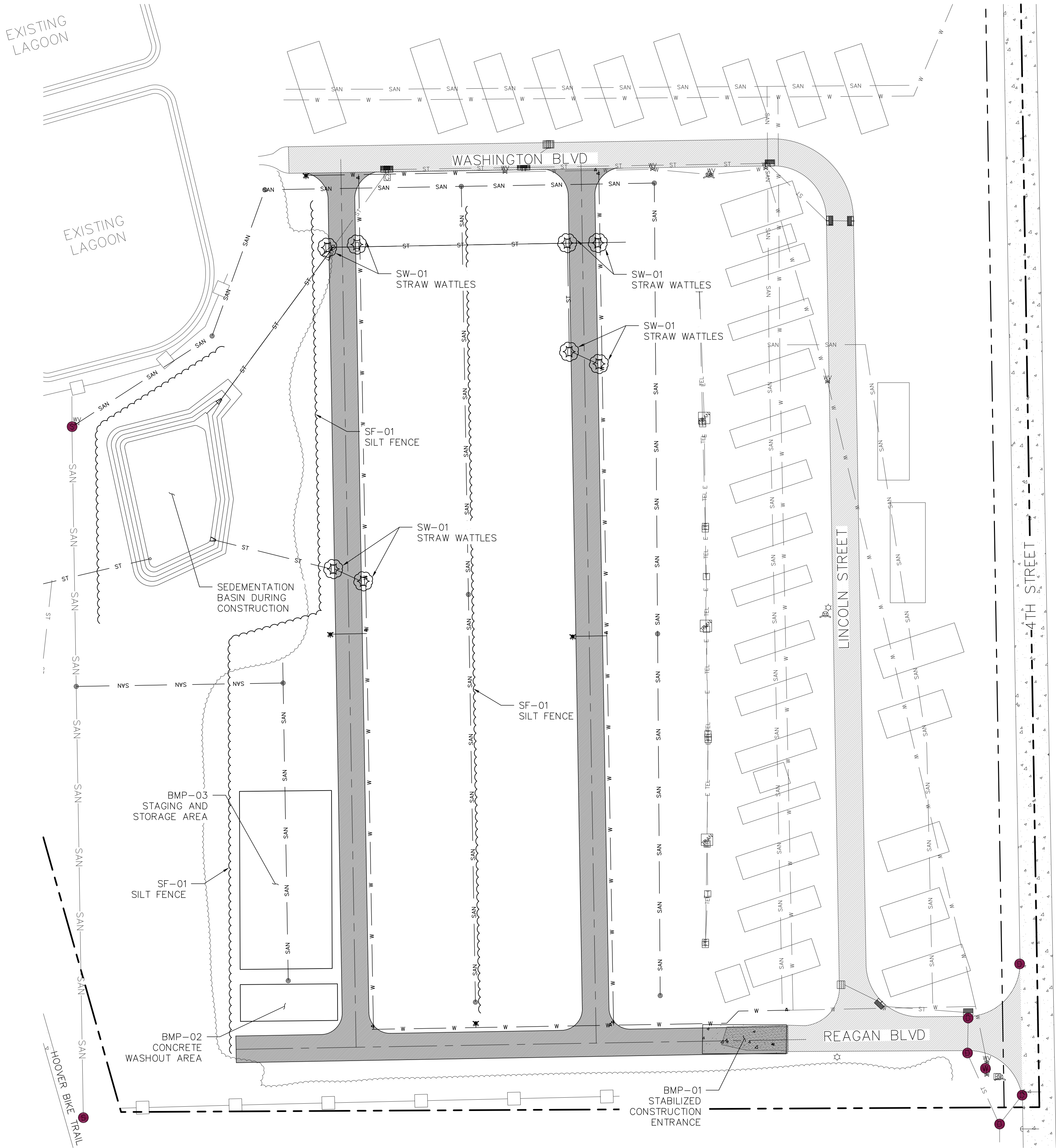
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SITE PLAN
C.01

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DIVISION 9-EROSION CONTROL NOTES

THE CONTRACTOR SHALL INSTALL AND MAINTAIN ALL SOIL EROSION MEASURES IN ACCORDANCE WITH THE STANDARDS AND SPECIFICATIONS SET FORTH IN THE MOST RECENT VERSION OF SUDAS AND THE IOWA STORM WATER MANUAL AND AS DIRECTED BY THE ENGINEER IN THE FIELD. THE INTENT IS FOR THE PREVENTION OF ERODED SOIL FROM EXITING THE SITE OR ENTERING LOCAL STORM SEWERS OR ADJACENT WATERWAYS. THE FOLLOWING ARE ACTIVITIES THAT THE CONTRACTOR SHALL CONDUCT ON A REGULAR BASIS DURING THE COURSE OF THE ON-SITE CONSTRUCTION ACTIVITIES.

1. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
2. SOIL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF UPLAND DISTURBANCE.
3. INFORMATION OUTSIDE THE PROJECT BOUNDARY IS FOR INFORMATION ONLY.
4. PERMANENT SOIL STABILIZATION SHALL BE DONE WITHIN 14 DAYS AFTER COMPLETION OF FINAL GRADING OF THE SOIL.
5. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.
6. EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE GOVERNING AGENCIES.
7. ALL TEMPORARY ROADWAYS, ACCESS DRIVES AND PARKING AREAS MUST BE STABILIZED AND BE OF SUFFICIENT WIDTH AND LENGTH TO PREVENT SEDIMENT FROM BEING TRACKED ONTO PUBLIC OR PRIVATE ROADWAYS. ANY SEDIMENT REACHING A PUBLIC OR PRIVATE ROAD SHALL BE REMOVED BY STREET CLEANING (NOT BY WATER FLUSHING) AS NECESSARY, OR BEFORE THE END OF EACH WORK DAY.
8. TIRES AND WHEEL WELLS OF VEHICLES AND CONSTRUCTION EQUIPMENT SHALL BE FREE OF DIRT AND/OR SEDIMENT BEFORE LEAVING A CONSTRUCTION AREA TO PREVENT TRACKING ONTO A PUBLIC OR PRIVATE PAVED ROAD OR SIDEWALK.
9. TRUCKS LOADED WITH MATERIAL THAT MAY BE CARRIED OFF BY WIND OR RAIN SHALL BE COVERED PRIOR TO LEAVING THE CONSTRUCTION SITE.
10. ALL ONSITE DRAINAGE STRUCTURES WITHIN THE CONSTRUCTION AREA AND DOWN SLOPE WITHIN THE PUBLIC RIGHT-OF-WAY SHALL BE PROTECTED WITH SEDIMENT CONTROL MEASURES.
11. THE DISCHARGE OF SEDIMENT INTO THE SEWER SYSTEM, AS PART OF SITE DEWATERING, MUST BE CONTROLLED AND MINIMIZED TO PREVENT THE CLOGGING OF THE SEWER SYSTEM.
12. THE USE, STORAGE AND DISPOSAL OF CHEMICALS, CEMENT AND OTHER COMPOUNDS AND BUILDING MATERIALS USED ON THE CONSTRUCTION SITE SHALL BE MANAGED DURING THE CONSTRUCTION PERIOD, TO PREVENT THEIR ENTRANCE INTO THE SEWER SYSTEM.

BMP IDENTIFICATION NUMBERS	
SF-01	SILT FENCE
SW-01	STRAW WATTLE AROUND INLETS
BMP-01	STABILIZED CONSTRUCTION ENTRANCE
BMP-02	CONCRETE WASH OUT AREA
BMP-03	STORAGE & STAGING AREA

SEQUENTIAL EROSION CONTROL PLAN:

1. CONSTRUCT STABILIZED ENTRANCE. CONTRACTOR SHALL CLEAN ACCESS CONNECTIONS AT THE END OF EACH DAY OF CONSTRUCTION.
2. PLACE SILT FENCE AT PERIMETER LOCATIONS SHOWN PRIOR TO THE START OF GRADING OPERATIONS. SILT FENCE SHALL BE MAINTAINED THROUGHOUT GRADING OPERATIONS.
3. CONSTRUCT TEMPORARY SEDIMENTATION BASIN AS PART OF INITIAL ROUGH GRADING OF SITE.
4. SEED ALL DISTURBED AREAS AT THE COMPLETION OF GRADING OPERATIONS.

TEMPORARY SEED:

1. CONTRACTOR SHALL COMPLETE TEMPORARY EROSION CONTROL AS SOON AS GRADING OPERATIONS ARE COMPLETE. TEMPORARY SEEDING SHALL BE AS FOLLOWS:
 - MARCH 1 - JUNE 1: 3 BUSHEL/acre OF OATS.
 - JUNE 1 - AUGUST 15: 35 LBS/acre OF GRAIN RYE.
 - AUGUST 15 - SEPTEMBER 15: 3 BUSHEL/acre OF OATS.
 - SEPTEMBER 15 - DECEMBER 1: 2 BUSHEL/acre OF GRAIN RYE.
2. FERTILIZER SHALL BE APPLIED AT A RATE OF 50-100-100 PER ACRE. MULCH SHALL BE STRAW OR HAY AND SHALL BE APPLIED AT A RATE OF 90-120 BALES/acre.

TOPSOIL NOTES:

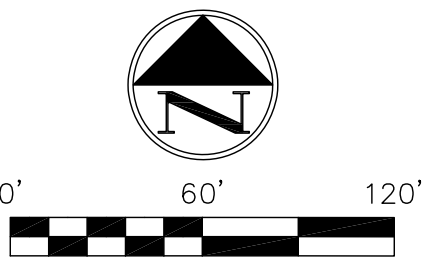
1. CONTRACTOR SHALL STOCKPILE AND RESPREAD A MINIMUM OF 4-INCHES OF EXISTING TOPSOIL ONSITE.

PERMANENT SEED:

1. ALL DISTURBED AREAS SHALL BE SEEDED, FERTILIZED AND MULCHED WITHIN 14 DAYS AFTER COMPLETION OF CONSTRUCTION.

DEMOLITION NOTES

1. COORDINATE ALL DEMOLITION ACTIVITIES WITH OWNER BEFORE COMMENCING ANY DEMOLITION WORK.
2. THE CONTRACTOR SHALL CONTACT ALL UTILITIES AND ARRANGE FOR THEIR LOCATION OF FACILITIES BEFORE BEGINNING DEMOLITION. THE CONTRACTOR SHALL COORDINATE WITH ALL PUBLIC UTILITIES FOR DISCONNECTION AND REMOVAL OF THEIR FACILITIES.
3. CLEARING AND REMOVAL OF TREES, BUSHES AND BRUSH SHALL INCLUDE REMOVAL FROM THE SITE OF ALL TRUNKS, BRANCHES, STUMPS AND ROOTS. THE CONTRACTOR SHALL REMOVE ALL STUMPS AND ROOTS.
4. WHERE WORK CAUSES REMOVAL OF PAVEMENT WITHIN EXISTING RIGHTS-OF-WAY, PAVEMENT SHALL BE REPLACED AS SOON AS PRACTICAL.
5. ADJACENT STREETS ARE TO BE MAINTAINED CLEAN AS PER CITY OF CORALVILLE REQUIREMENTS.
6. WHEN PEDESTRIAN TRAFFIC FLOW IS INTERRUPTED, CONTRACTOR SHALL INSTALL PROPER SIGNAGE ("SIDEWALK CLOSED AHEAD USE OTHER SIDE").
7. WHEN WORK EXTENDS INTO THE STREET, THE CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL CONFORMING TO THE CURRENT EDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD)".



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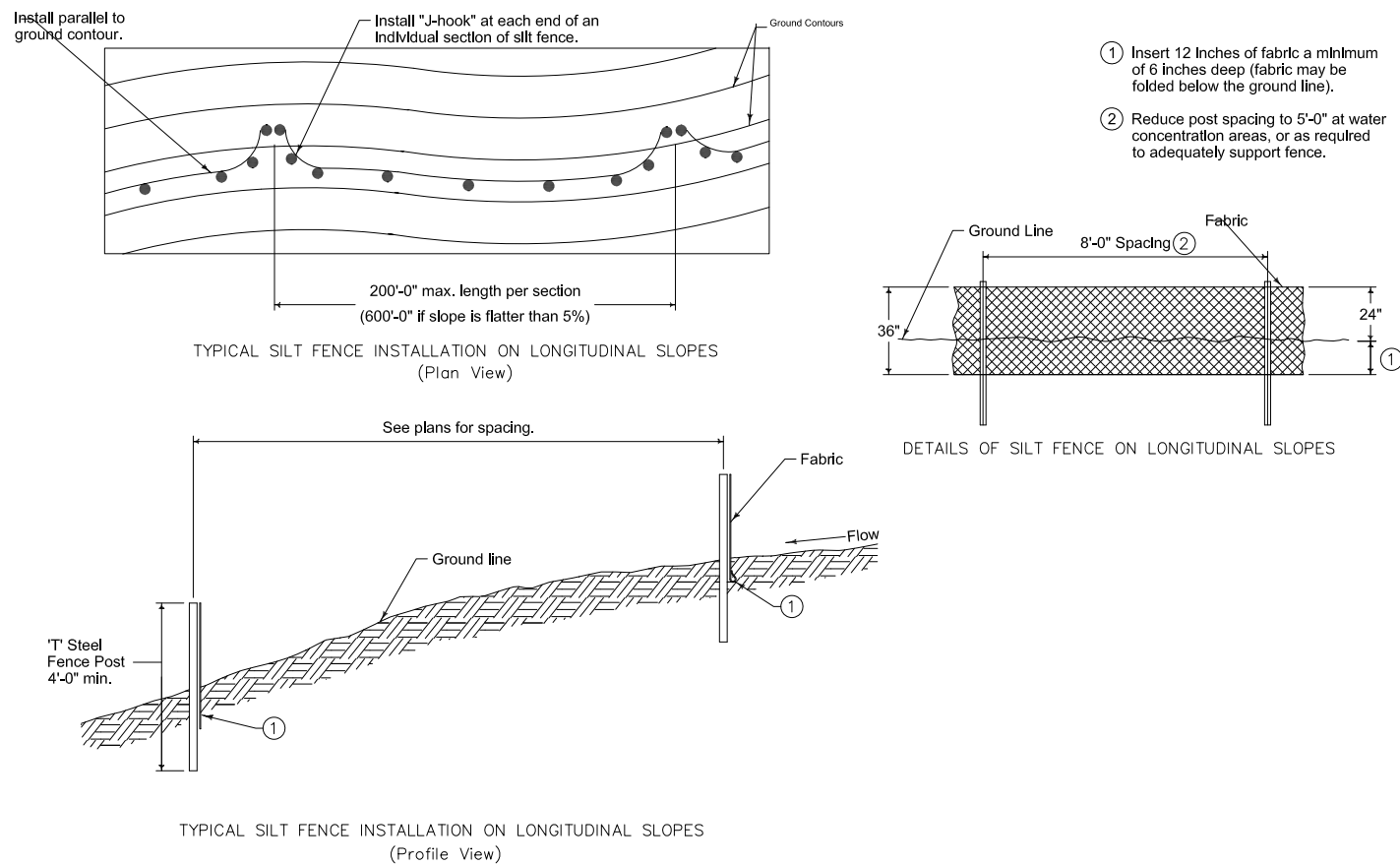
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EROSION CONTROL PLAN

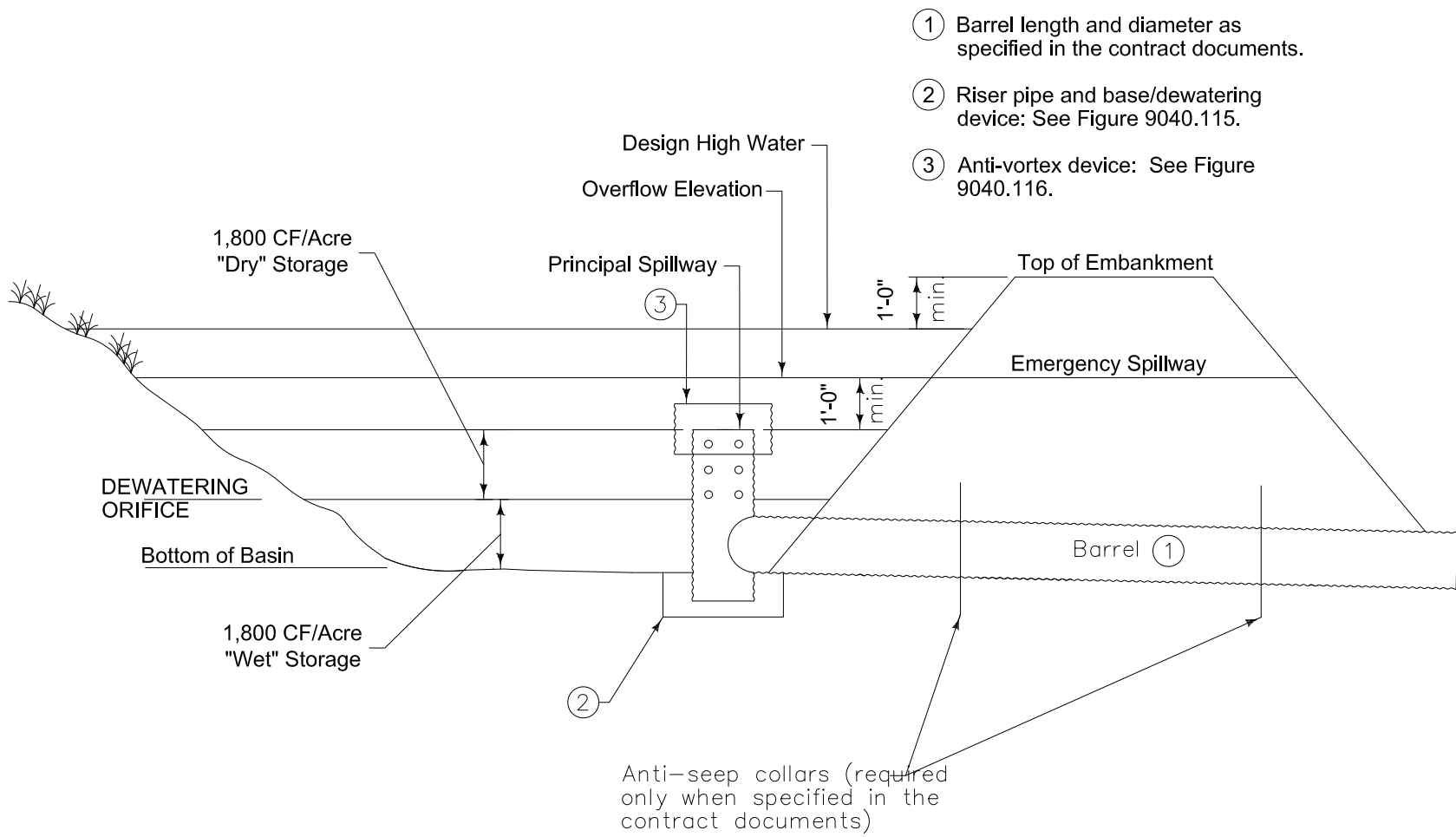
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STAGING NOTES

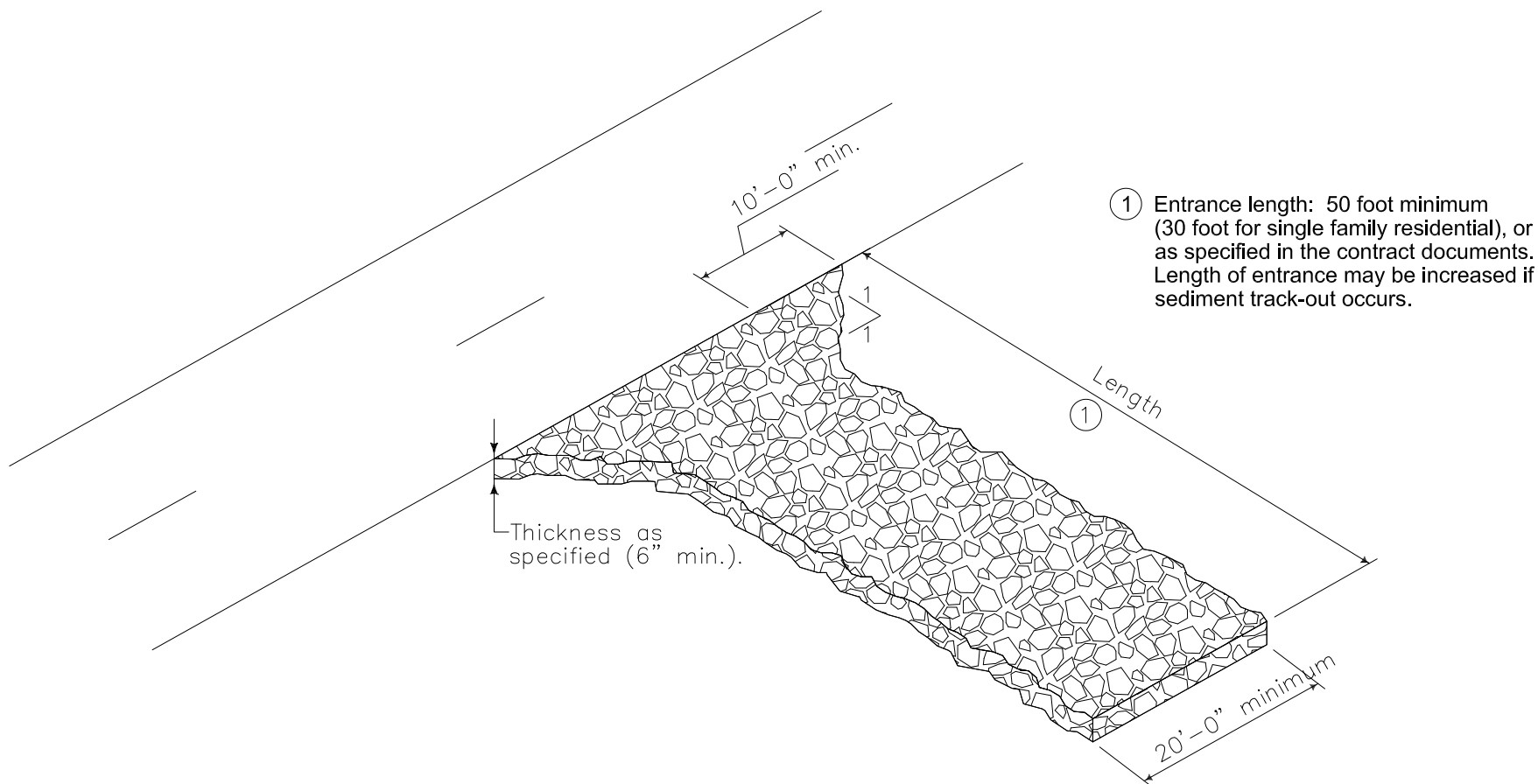
1. CONTRACTOR'S ACTIVITIES TO THE AREAS OF SUGGESTED STAGES ALONE. IT IS UNDERSTOOD THAT VARIOUS STEPS INCLUDED HEREIN MAY OCCUR SIMULTANEOUSLY. THE CONTRACTOR MAY CONDUCT SEVERAL OPERATIONS DO NOT CONFLICT WITH THE STAGING INDICATED HEREIN, OTHER PORTIONS OF THIS PROJECT, AND OTHER CONSTRUCTION ACTIVITIES.
2. THE CONTRACTOR SHALL COORDINATE WITH ALL PUBLIC AND PRIVATE PROJECTS IN THE AREA AT ALL TIMES.
3. IOWA CODE 480, UNDERGROUND FACILITIES INFORMATION, REQUIRES VERBAL NOTICE TO IOWA ONE CALL, 1-800-292-8989, NOT LESS THAN 48 HOURS BEFORE EXCAVATING, EXCLUDING WEEKENDS AND LEGAL HOLIDAYS.
4. THE UTILITY COMPANIES WHOSE FACILITIES ARE SHOWN ON THE PLANS OR KNOWN TO BE WITHIN CONSTRUCTION LIMITS, SHALL BE NOTIFIED BY THE CONTRACTOR PRIOR TO START OF CONSTRUCTION.
5. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE APPROVED WASTE AREAS OR DISPOSAL SITES FOR EXCESS MATERIAL (EXCAVATED MATERIAL OR BROKEN CONCRETE) WHICH IS NOT DESIRABLE TO BE INCORPORATED INTO THE WORK INVOLVED ON THIS PROJECT.
6. ALL HOLES RESULTING FROM OPERATIONS OF THE CONTRACTOR INCLUDING REMOVAL OF FENCE POSTS, UTILITY POLES, TREES OR FOUNDATION STRUCTURES SHALL BE FILLED AND CONSOLIDATED TO FINISHED GRADE AS DIRECTED BY THE PROJECT ENGINEER TO PREVENT FUTURE SETTLEMENT. THE VOIDS SHALL BE FILLED AS SOON AS PRACTICAL, PREFERABLY THE DAY CREATED AND NOT LATER THAN THE FOLLOWING DAY. HOLES NEAR PEDESTRIAN WALKWAYS SHALL BE FENCED OFF AT ALL TIMES AND IF PRACTICAL COVERED WITH BOARDS DURING THE OVERNIGHT HOURS AND AT TIMES WHERE WORK IS NOT TAKING PLACE AT THE LOCATION OF THE HOLE FOR AN EXTENDED PERIOD OF TIME. ANY PORTION OF THE RIGHT-OF-WAY OR PROJECT LIMITS (INCLUDING BORROW AREAS AND OPERATION AREAS) DISTURBED BY ANY SUCH OPERATIONS SHALL BE RESTORED TO AN ACCEPTABLE CONDITION. THIS OPERATION SHALL BE CONSIDERED INCIDENTAL TO OTHER BID ITEMS IN THE PROJECT.
7. DURING REMOVAL AND CONSTRUCTION, THE CONTRACTOR SHALL USE ALL MEANS NECESSARY TO CONTROL DUST SPREADING FROM ALL WORK AND STAGING AREAS. DUST CONTROL MEASURES SHALL BE IN ACCORDANCE WITH APPLICABLE SPECIFICATIONS AND APPROVED BY THE PROJECT ENGINEER AND SHALL BE INCIDENTAL.
8. EROSION CONTROL MEASURES NECESSARY TO PROTECT AGAINST SILTATION, EROSION AND DUST POLLUTION ON THE PROJECT SITE SHALL COMPLY WITH SOIL EROSION CONTROL REQUIREMENTS OF IOWA CODE AND LOCAL ORDINANCES.
9. THE CONTRACTOR SHALL TAKE SPECIAL CARE TO MAINTAIN POSITIVE DRAINAGE WHERE INTAKES ARE LOCATED.
10. THE CONTRACTOR SHALL MAINTAIN ACCESS TO INDIVIDUAL PROPERTIES THROUGHOUT THE CONSTRUCTION OF THE PROJECT. THE CONTRACTOR SHALL NOTIFY PROPERTY OWNERS 48 HOURS IN ADVANCE OF DRIVEWAY MODIFICATIONS THAT WILL TEMPORARILY LIMIT OWNER ACCESS TO THEIR PROPERTY.
11. THE CONTRACTOR SHALL CONFINE WORK TO THE CONSTRUCTION LIMITS AND RIGHT-OF-WAY. IF THE CONTRACTOR OBTAINS ADDITIONAL EASEMENTS FOR THE STORAGE OF EQUIPMENT AND MATERIALS, COPIES OF THE AGREEMENTS WITH THE PROPERTY OWNERS SHALL BE PROVIDED TO THE PROJECT ENGINEER.
12. THE CONTRACTOR SHALL NOT DISTURB GRASS AREAS OR TREES OUTSIDE THE CONSTRUCTION LIMITS. THE CONTRACTOR WILL NOT BE PERMITTED TO PARK OR SERVICE VEHICLES OR EQUIPMENT OR USE THESE AREAS FOR STORAGE OF MATERIALS. ALL STORAGE, PARKING AND SERVICE AREA(S) WILL BE SUBJECT TO THE APPROVAL OF THE PROJECT ENGINEER.
13. DO NOT DRIVE ON THE PAVEMENT WITH CONSTRUCTION TRAFFIC GREATER THAN THE LEGAL LOAD LIMITS.



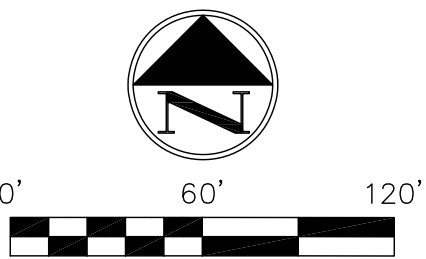
SILT FENCE (9040-119)



SEDIMENT BASIN WITHOUT
EMERGENCY SPILLWAY (9040.113)



STABILIZED CONSTRUCTION
ENTRANCE (9040.120)



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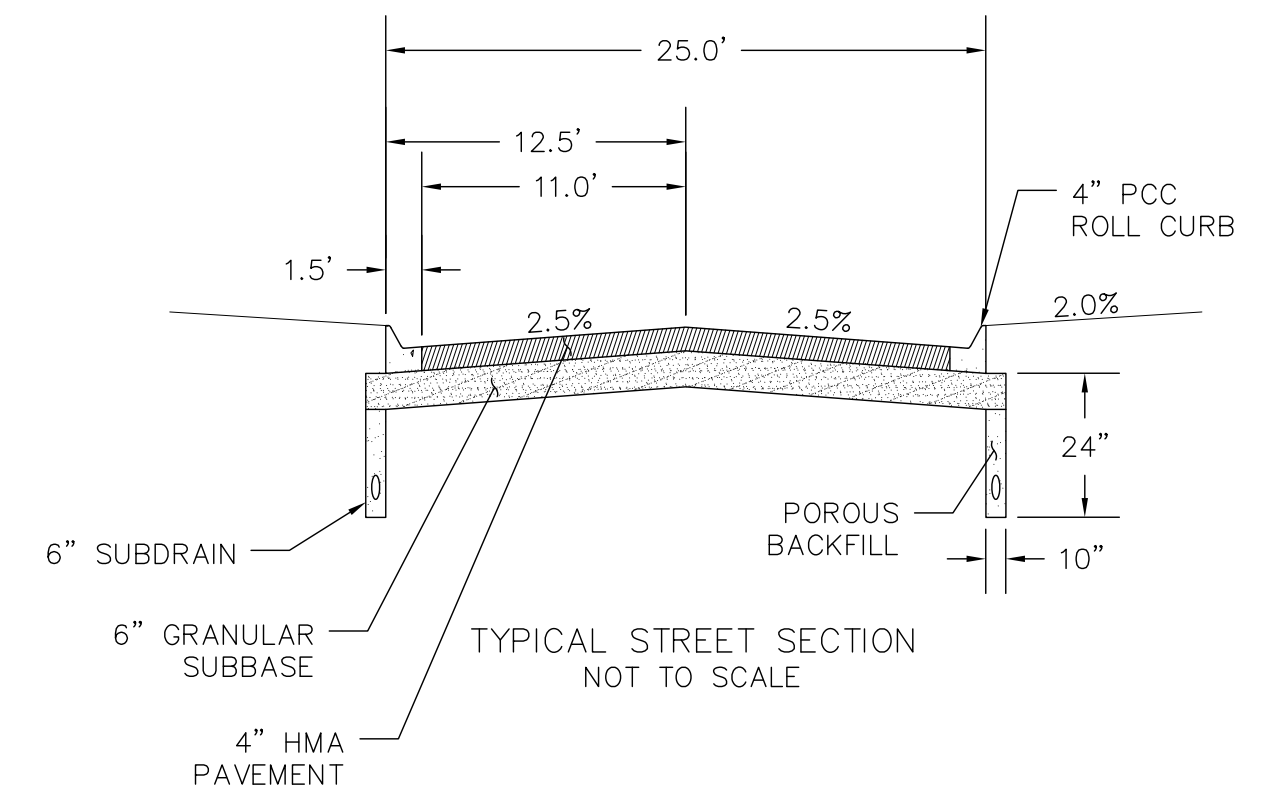
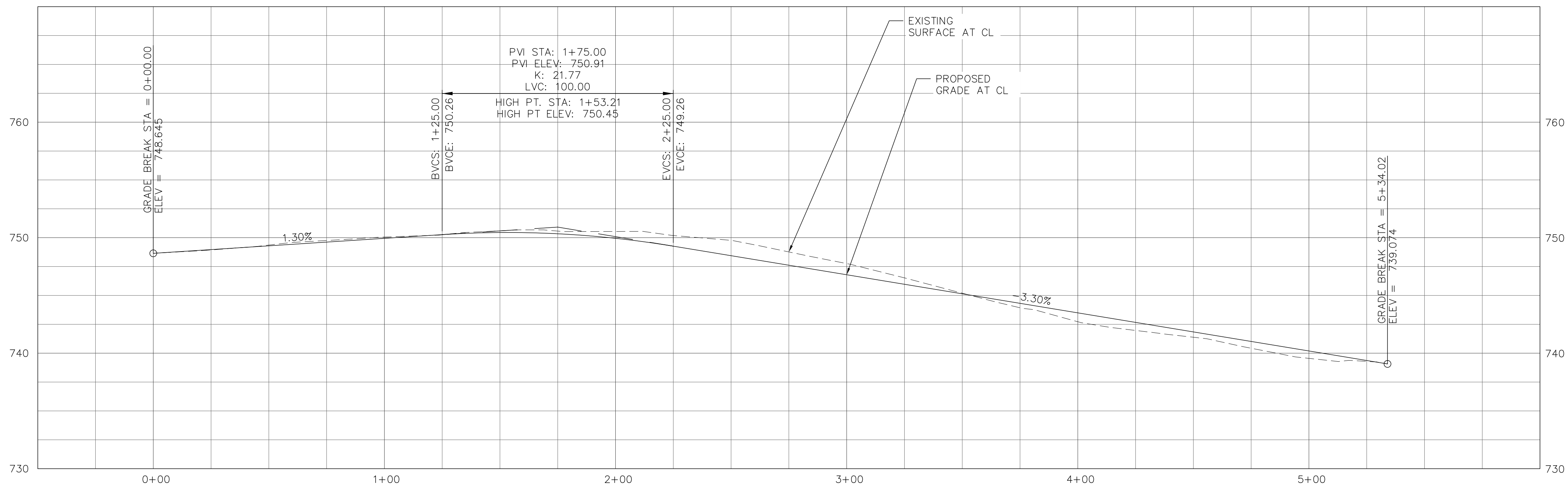
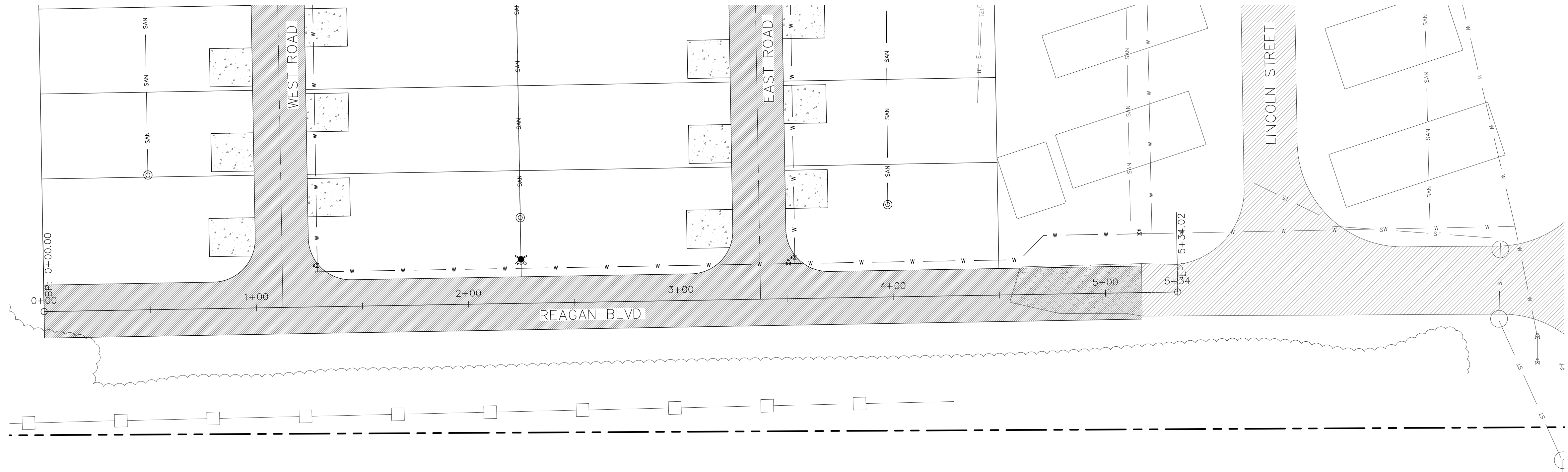
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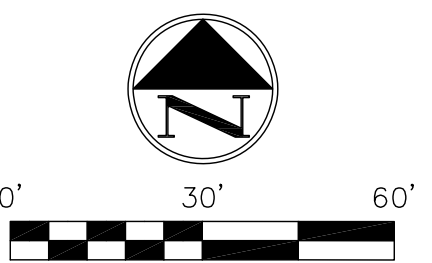
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SHEET NO.
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PAVING CONSTRUCTION NOTES:

1. A MINIMUM 6" THICK GRANULAR SUBBASE OF I.D.O.T. GRADATION 12 OR 14 SHALL BE PROVIDED FOR ALL PUBLIC AND PRIVATE STREETS. SUBBASE SHALL BE IN ACCORDANCE WITH SUDAS SPECIFICATION SECTION 2010.
2. NO NEW CONCRETE PAVING SHALL BE OPENED TO VEHICULAR TRAFFIC UNTIL APPROVED BY THE CITY OF WEST BRANCH.
3. THE CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL PER IDOT STANDARD ROAD PLAN AND CITY OF WEST BRANCH REQUIREMENTS AT ALL TIMES DURING WORK WITHIN PUBLIC R.O.W.



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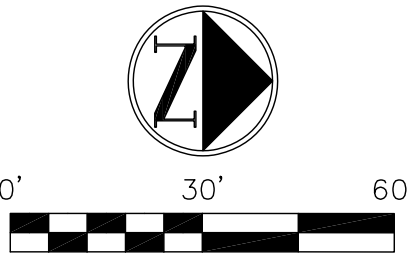
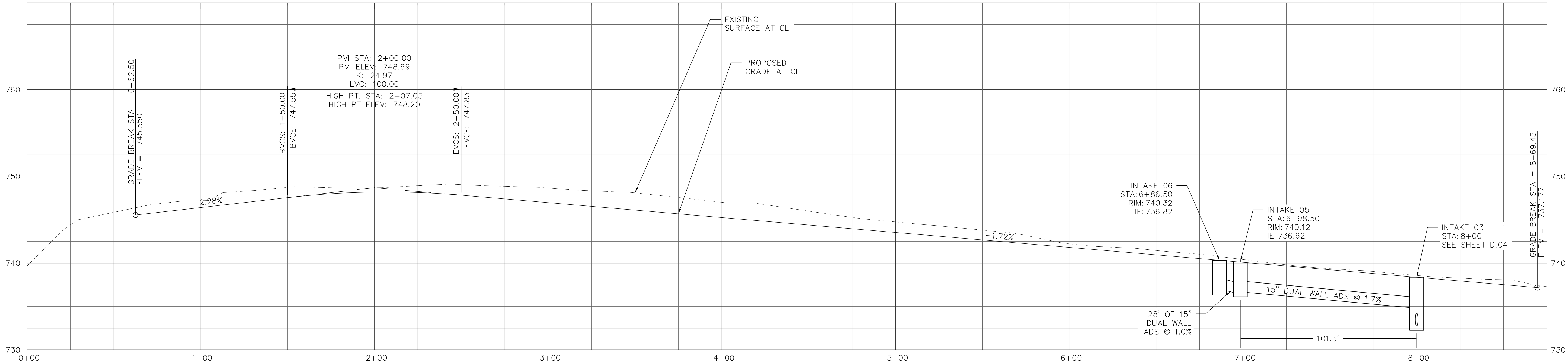
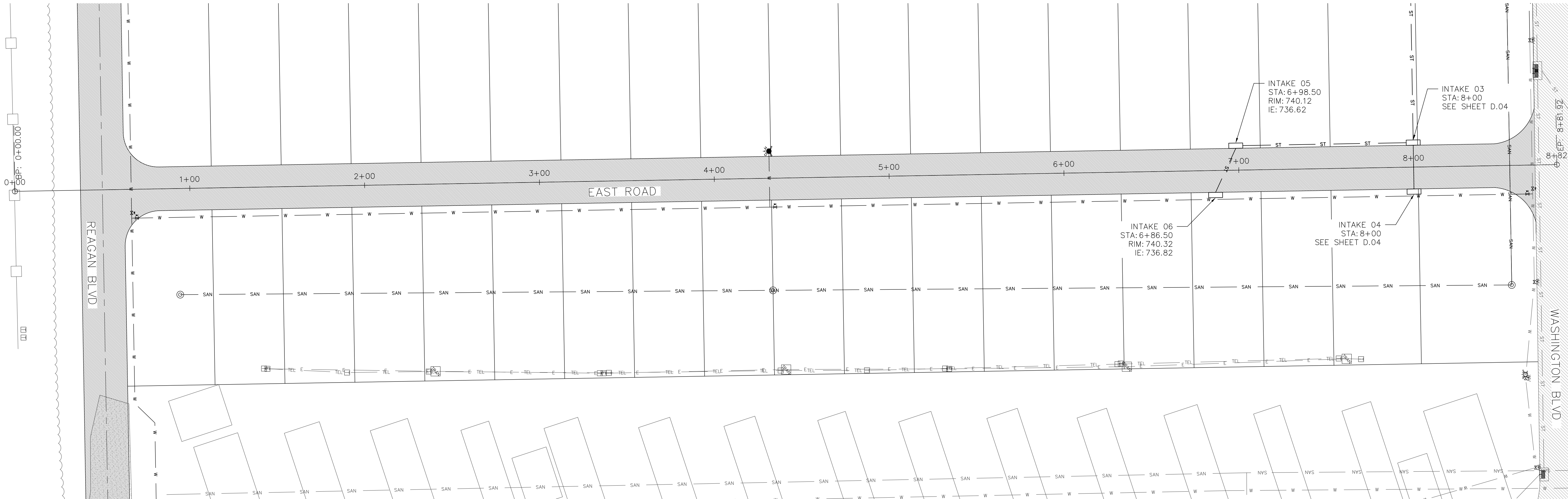
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ROADWAY AND DRAINAGE PLANS
SOUTH ROAD

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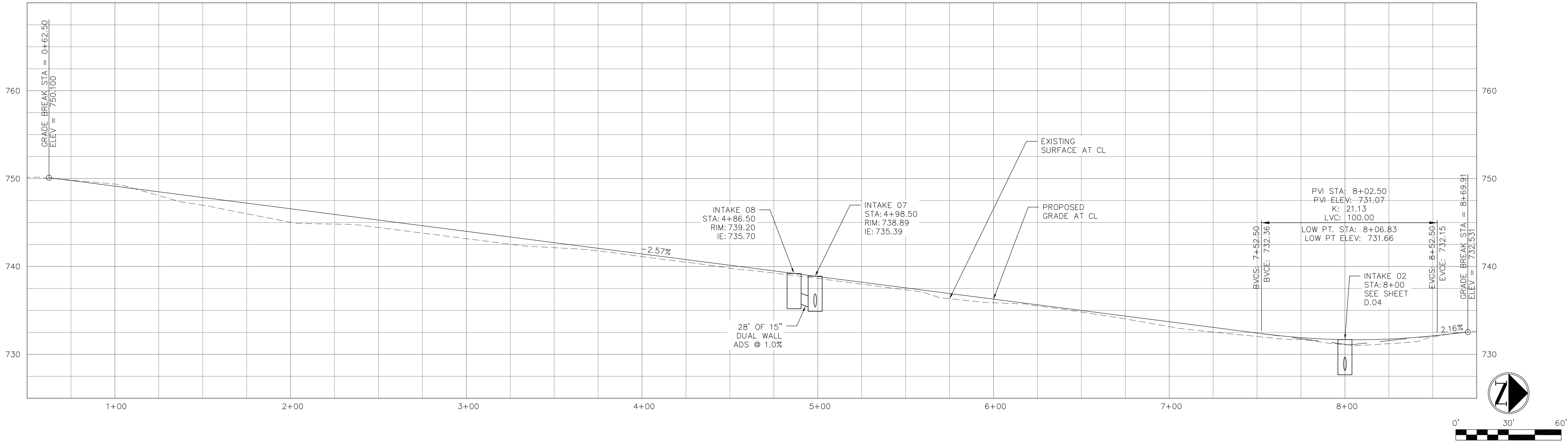
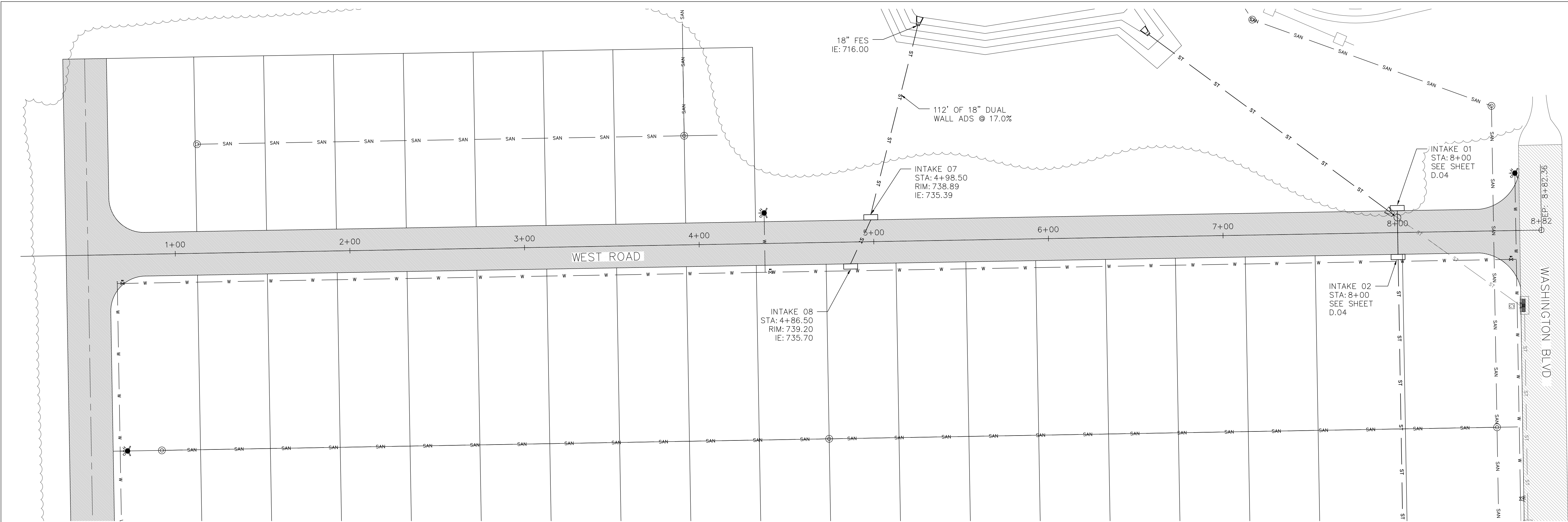
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ROADWAY AND DRAINAGE PLANS
EAST ROAD

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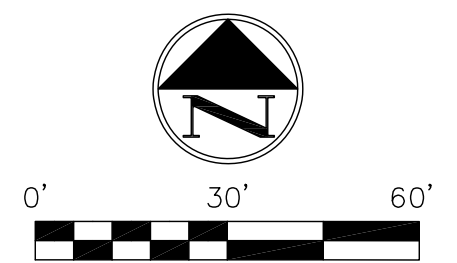
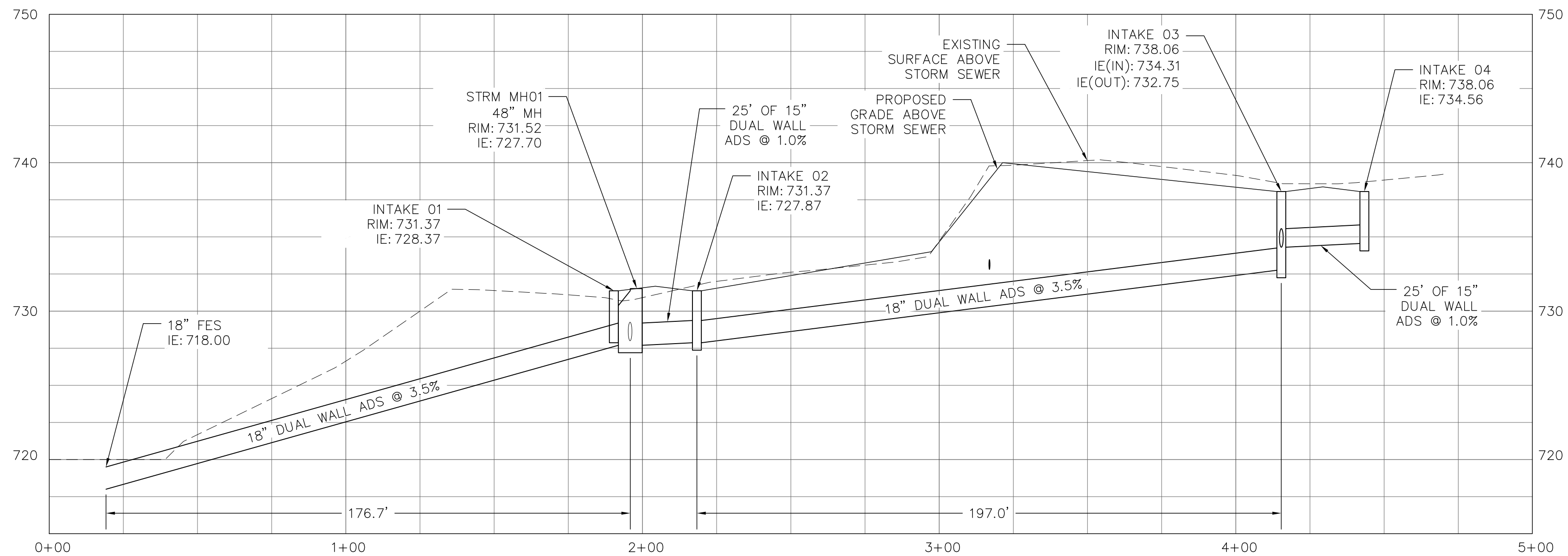
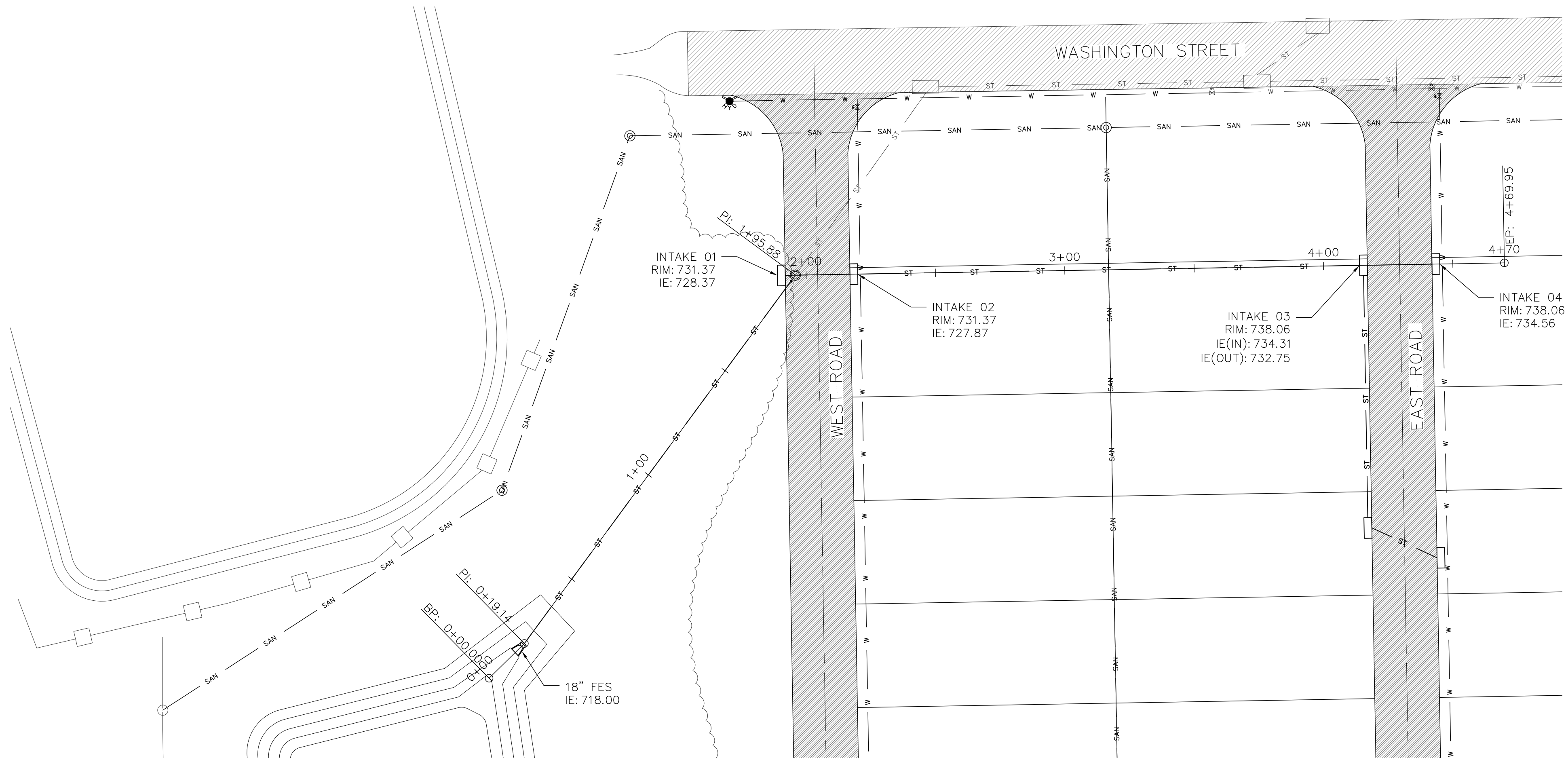
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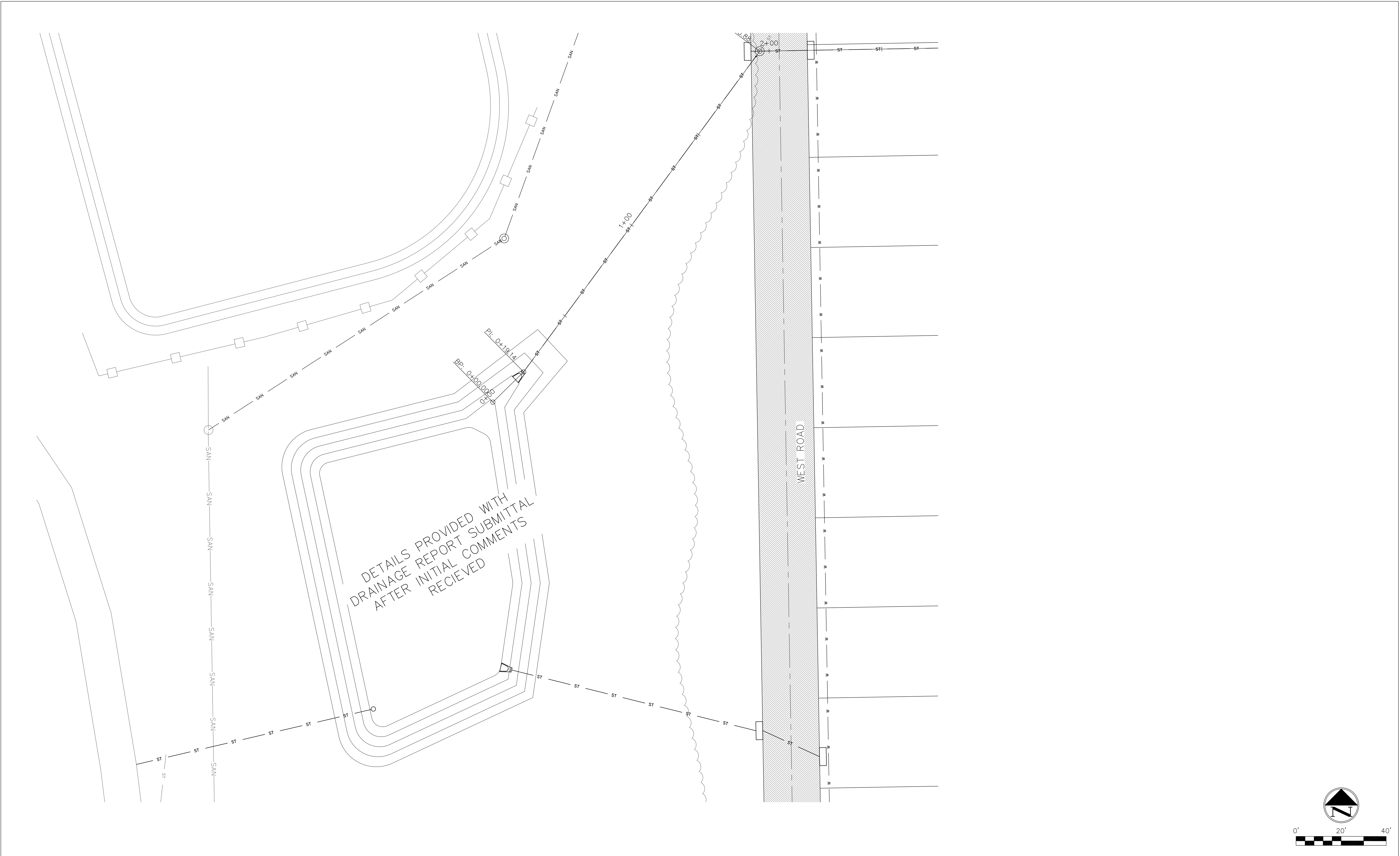
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ROADWAY AND DRAINAGE PLANS
STORM SEWER TO BASIN

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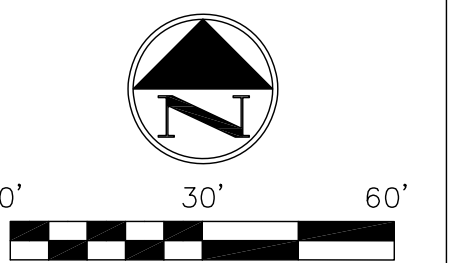
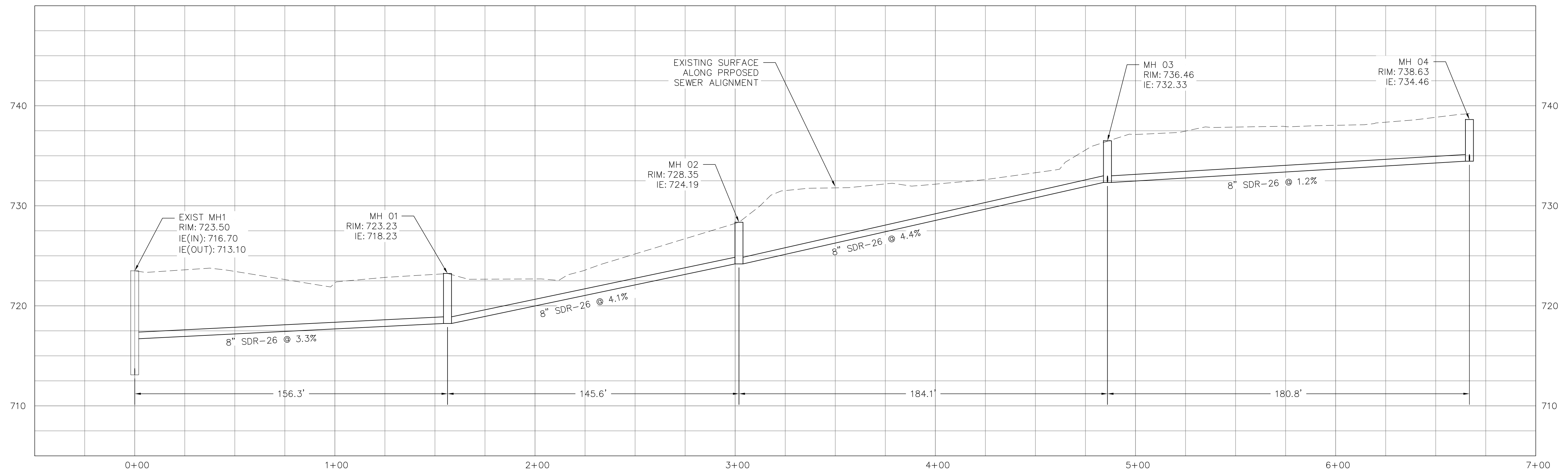
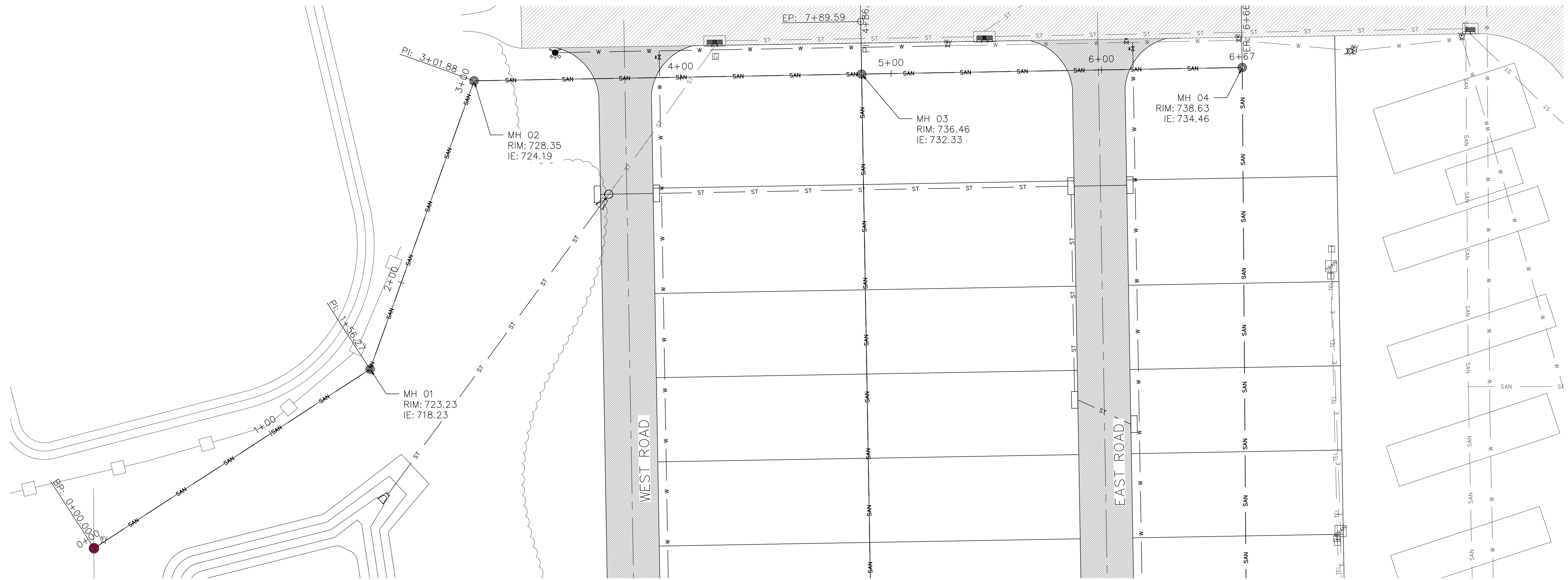
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ROADWAY AND DRAINAGE PLANS
DRAINAGE BASIN DETAIL

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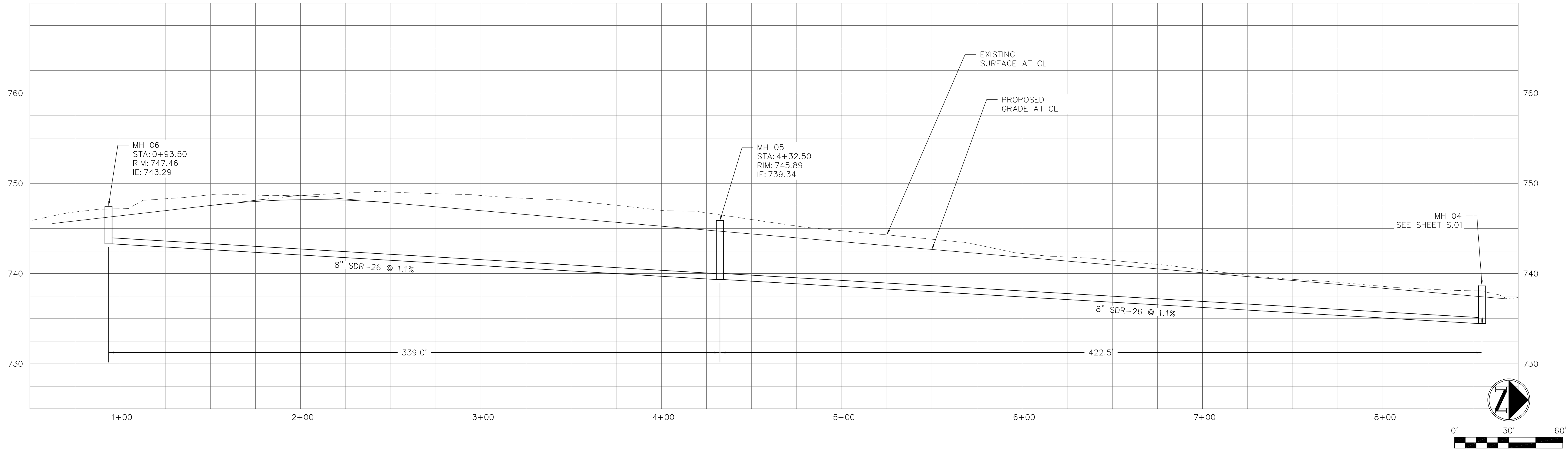
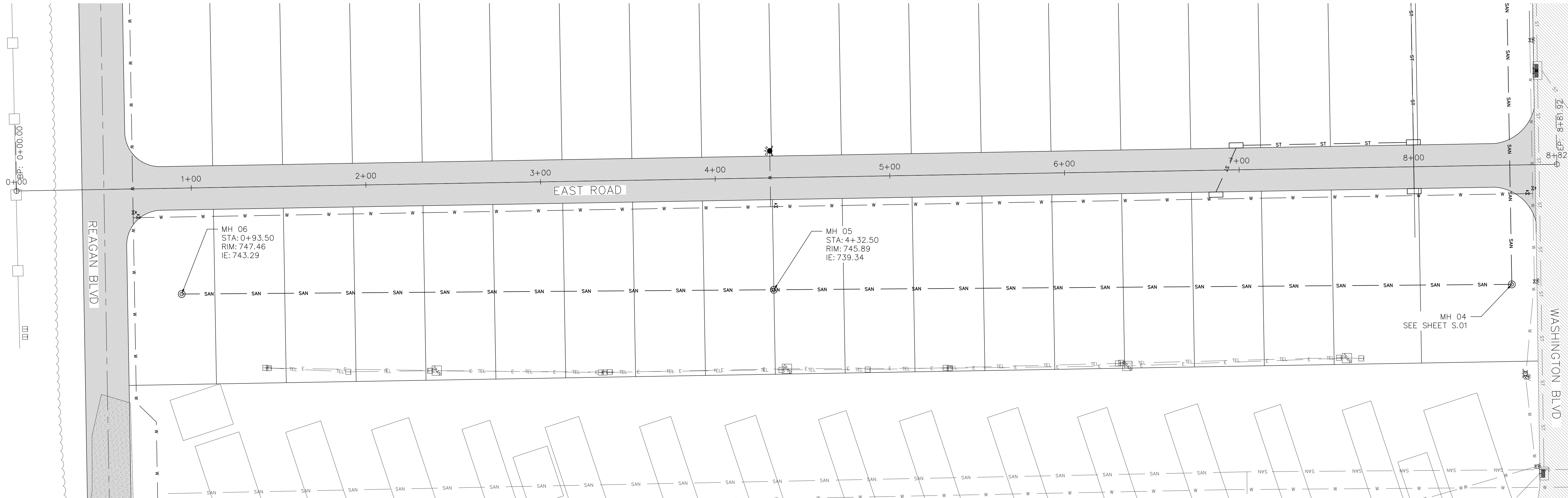
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SANITARY SEWER MAIN PLANS
WEST ROAD

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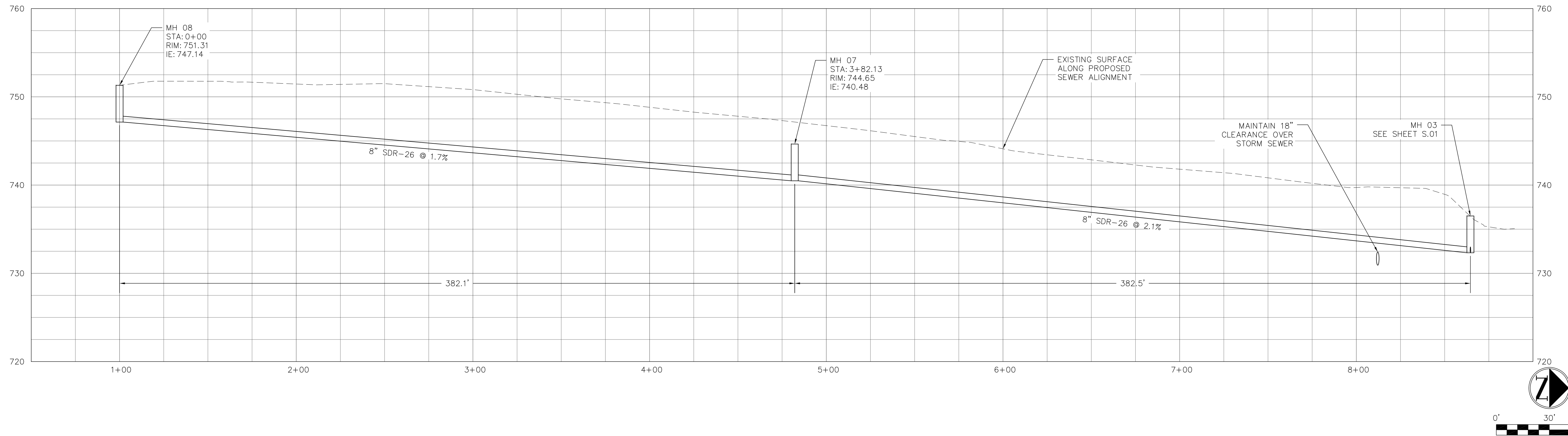
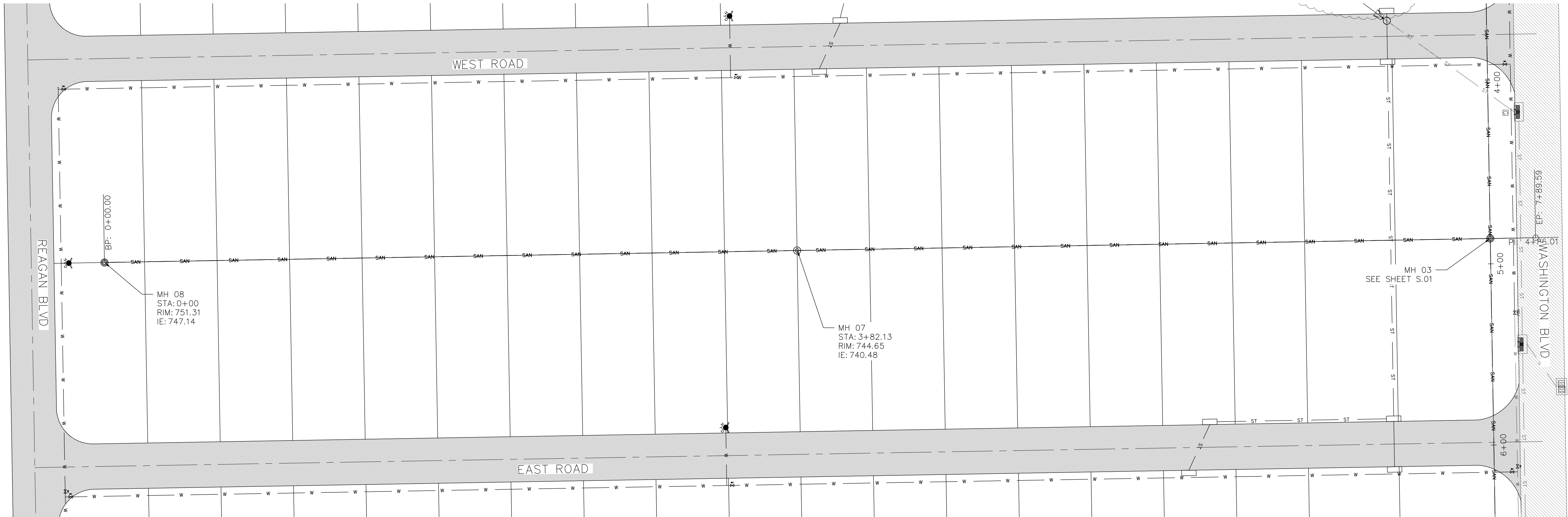
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SANITARY SEWER MAIN PLANS
EAST ROAD

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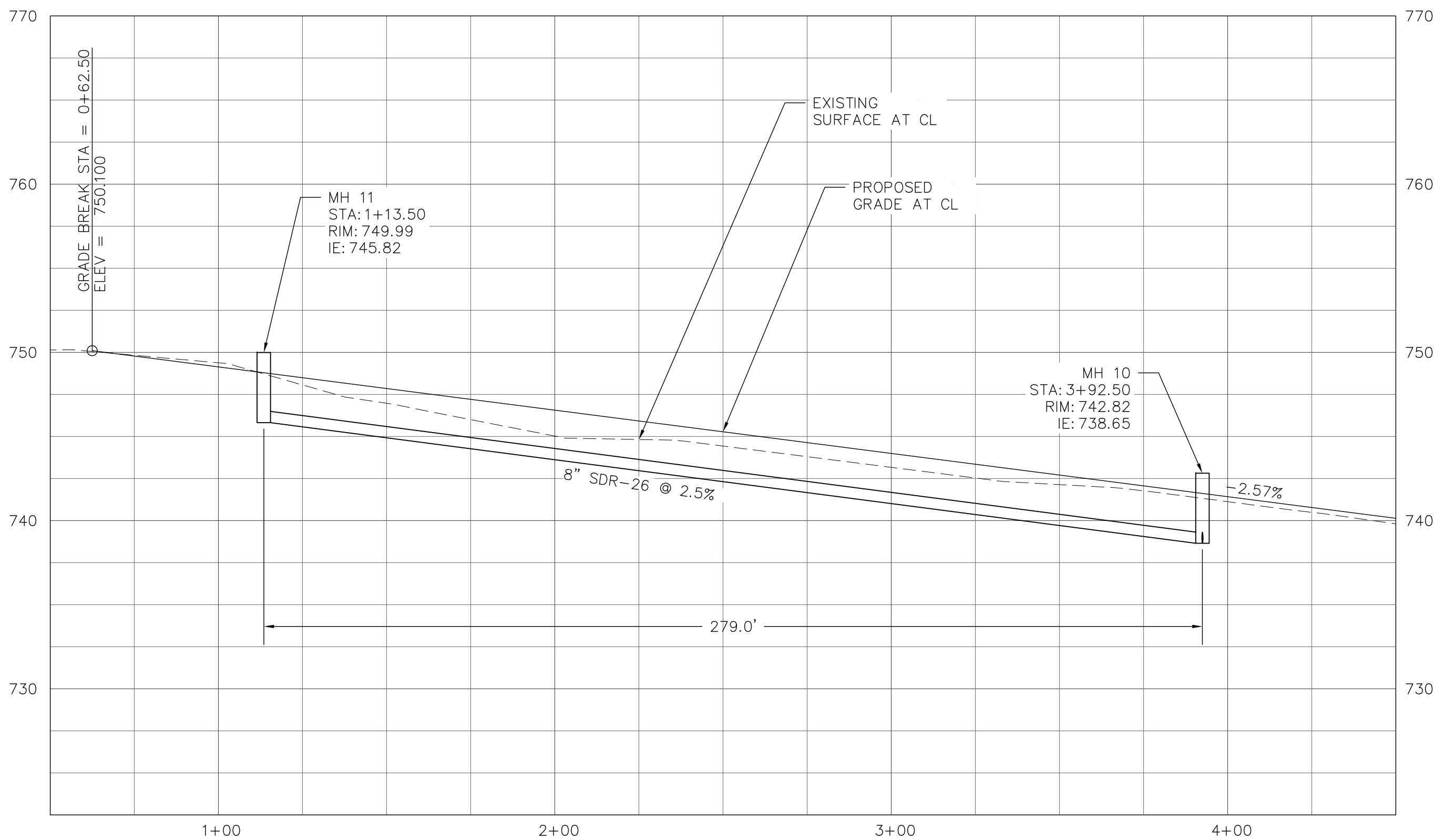
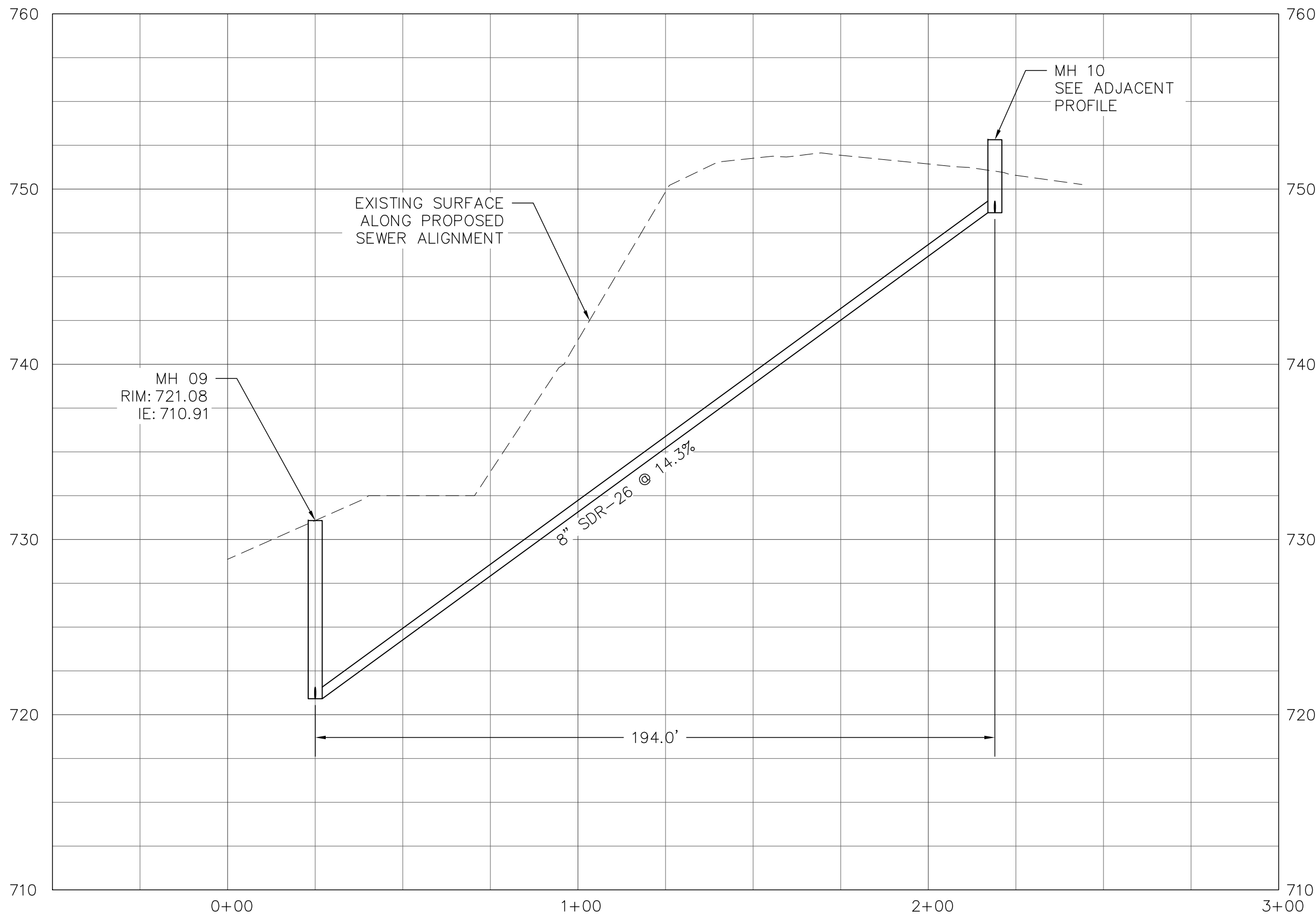
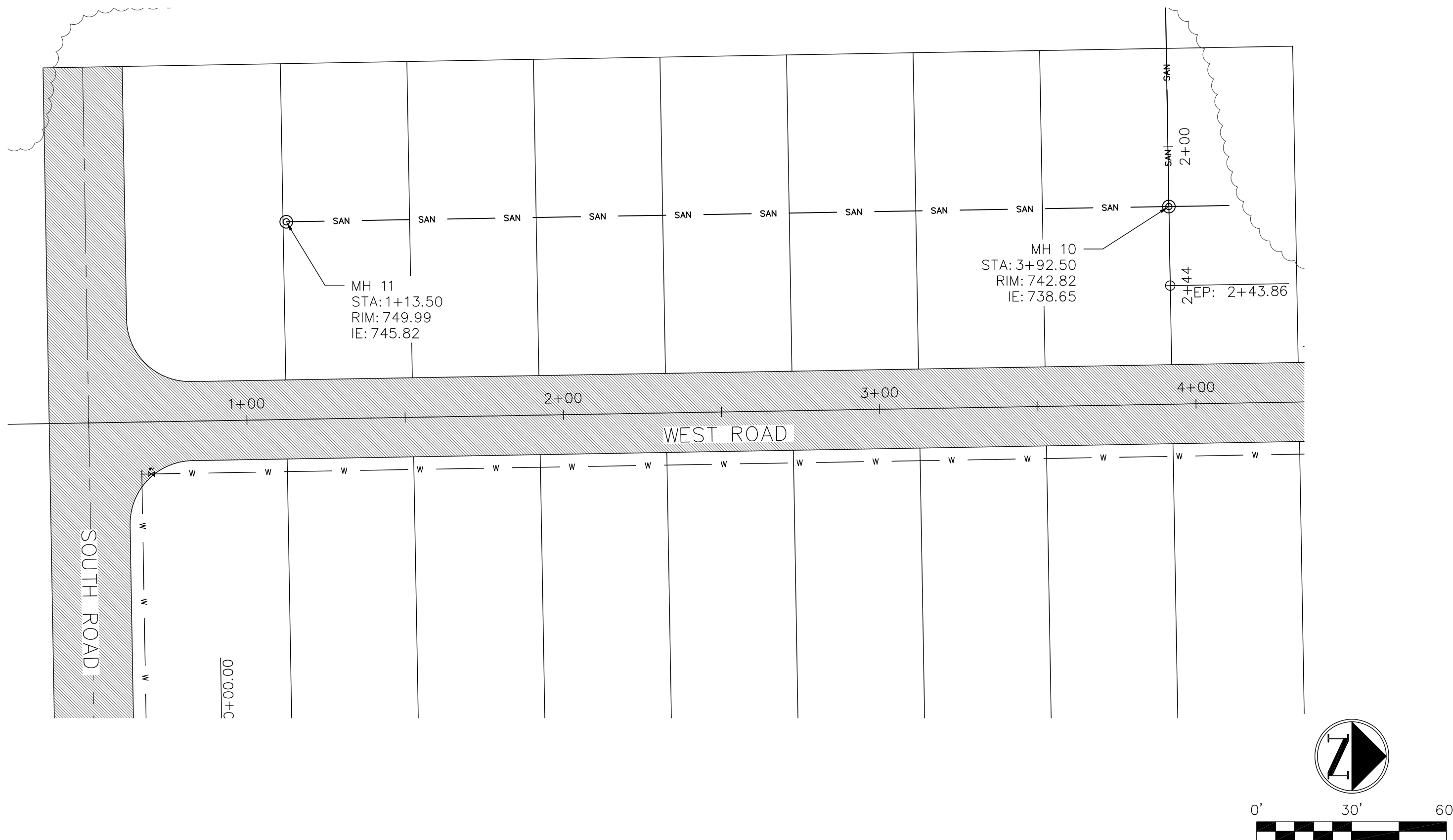
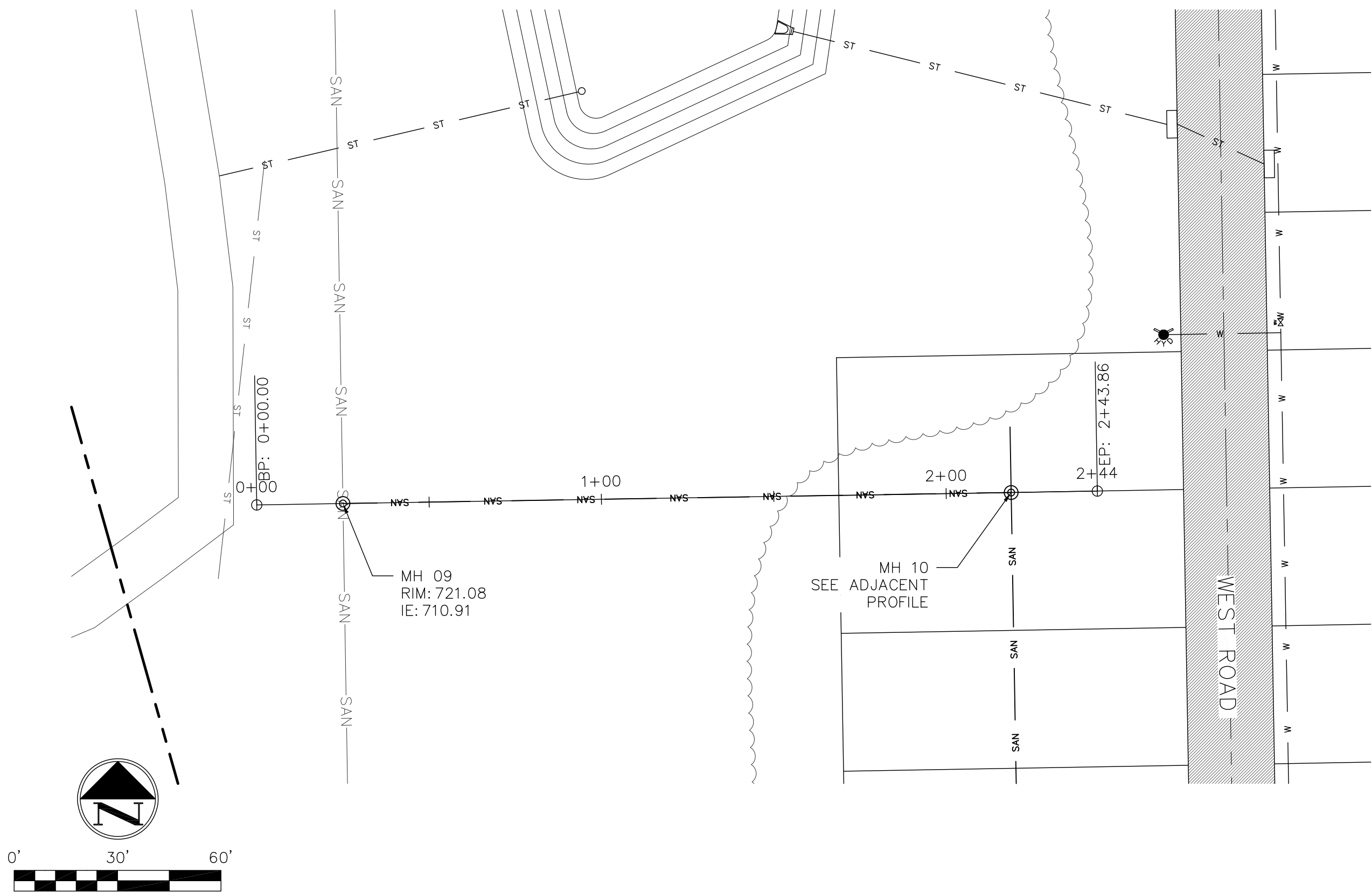
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SANITARY SEWER MAIN PLANS
BACK OF LOTS

SHEET NO.
S.03



*WEST ROAD STATIONING USED FOR STATION/OFFSET POINTS OF SEWER MAIN.

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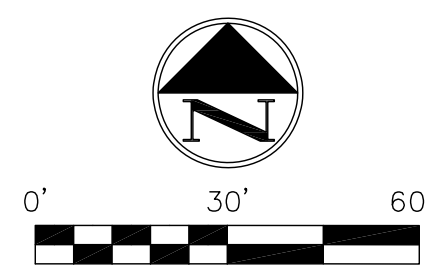
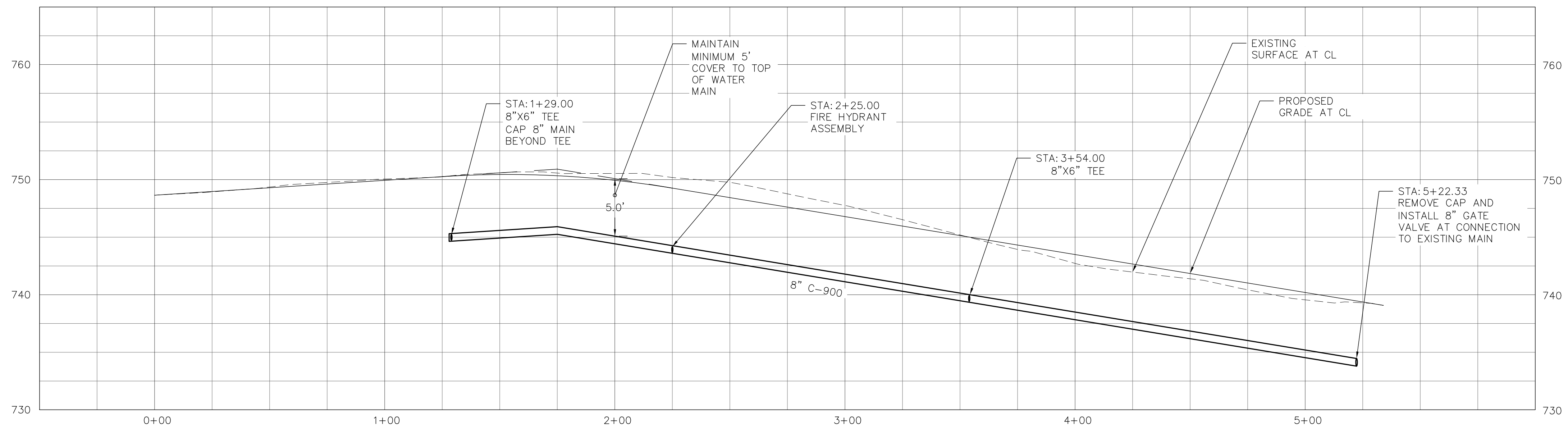
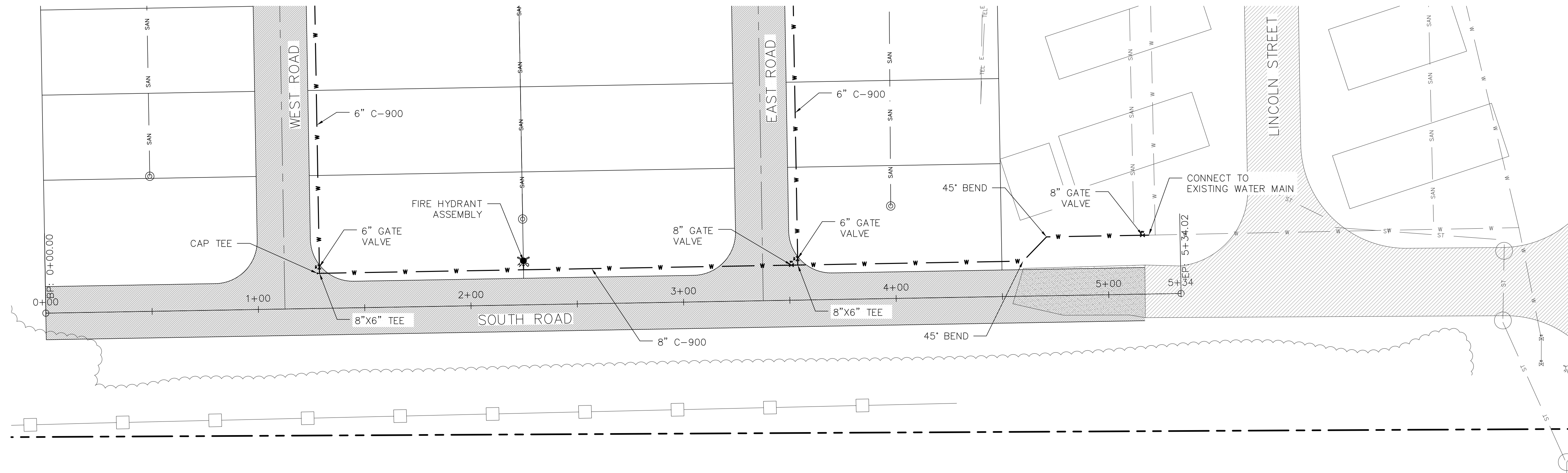
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SANITARY SEWER MAIN PLANS
WEST ROAD

SHEET NO.
S.04



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 DATE: May 06, 11:36pm
WEST BRANCH VILLAGE IMP PLANS.dwg | W.01

NO.	REVISION DESCRIPTION	APPROVED	DATE

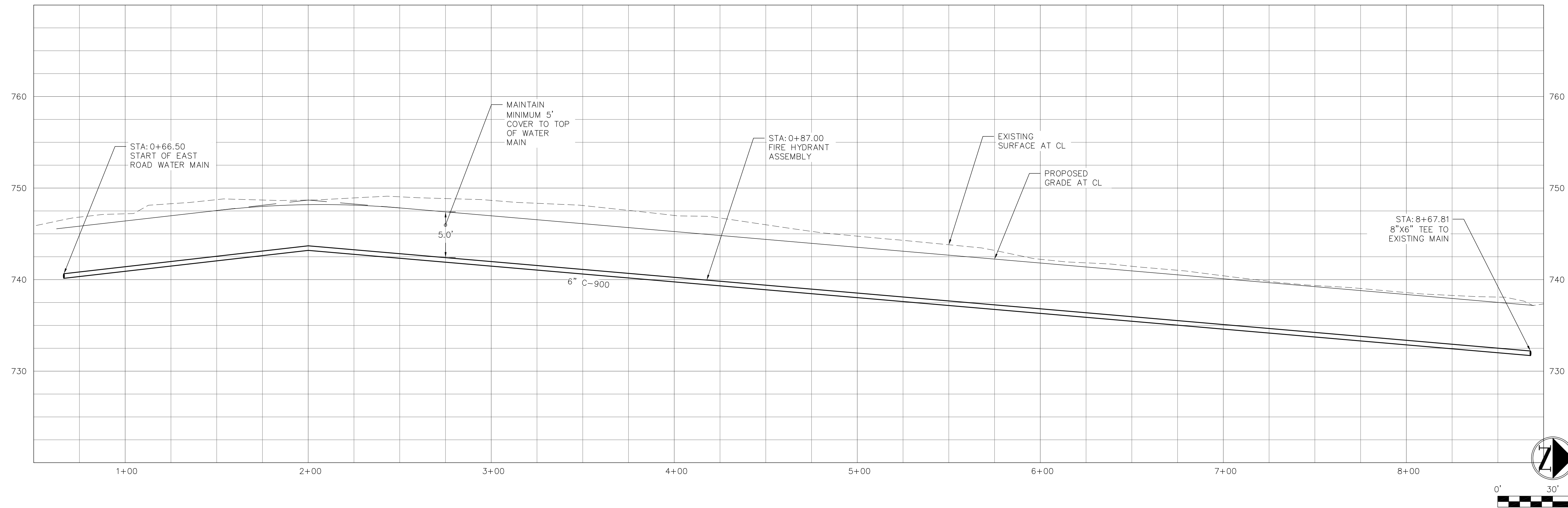
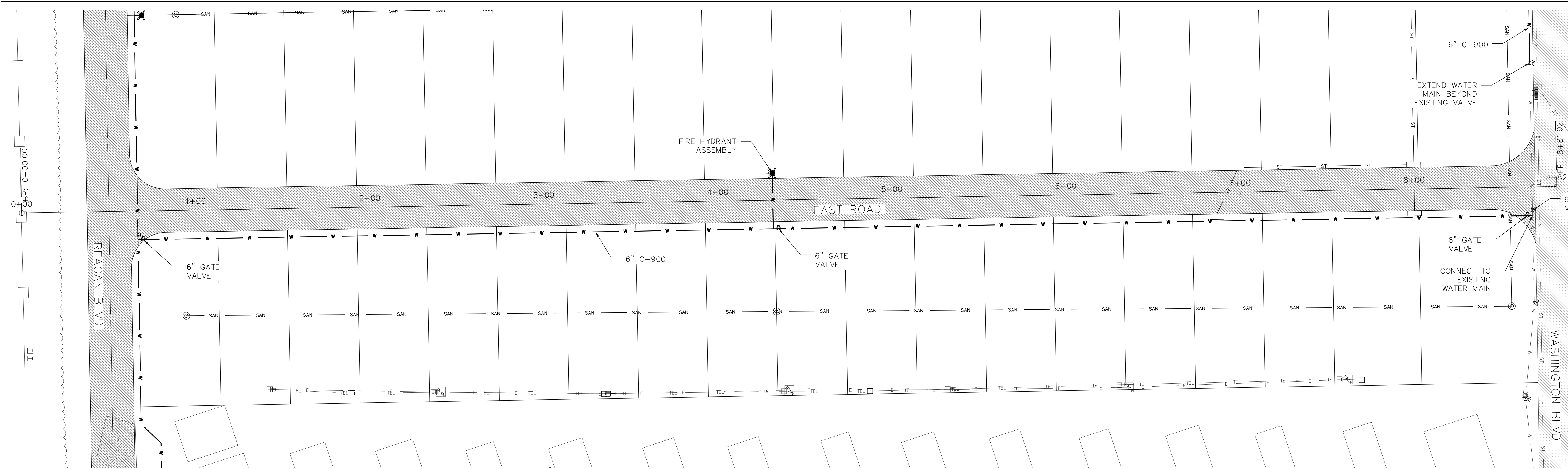
CLEARWELL

2135 BLAKE BLVD SE
 CEDAR RAPIDS, IOWA 52403
 319-654-4911
 CLEARWELLEN.G.COM

WEST BRANCH VILLAGE, LLC
WEST BRANCH VILLAGE EXPANSION
WEST BRANCH, IOWA

WATER MAIN PLANS
SOUTH ROAD

SHEET NO.
W.01



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DATE: May 06, 11:36pm
WEST BRANCH VILLAGE IMP PLANS.dwg | W.02

NO.	REVISION DESCRIPTION	APPROVED	DATE

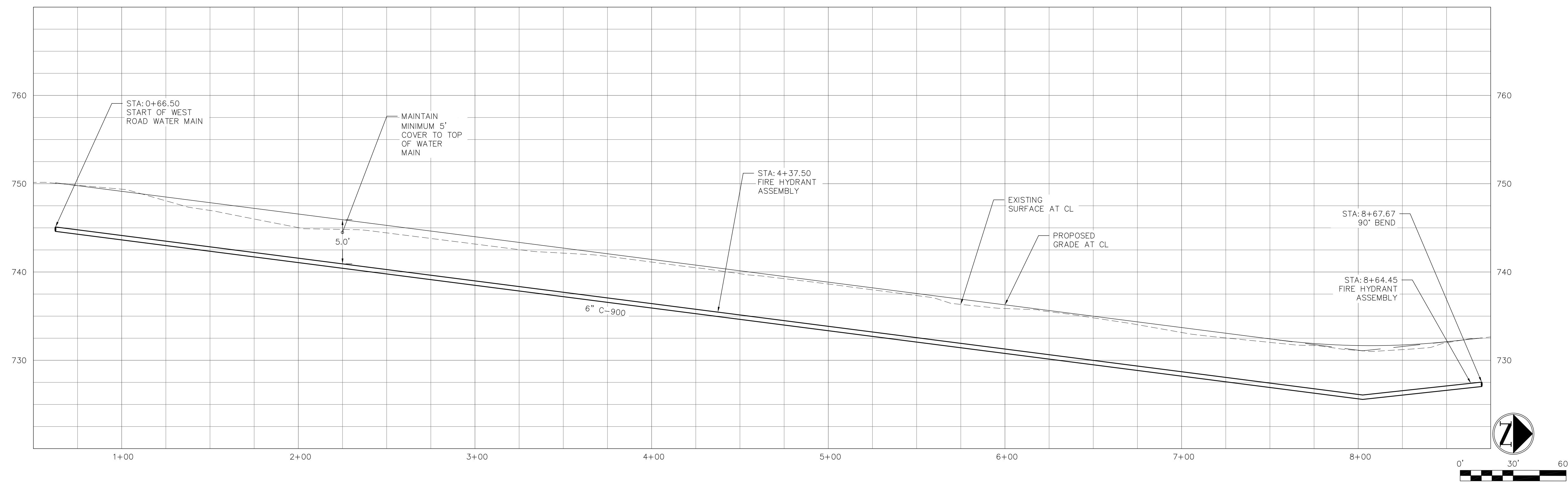
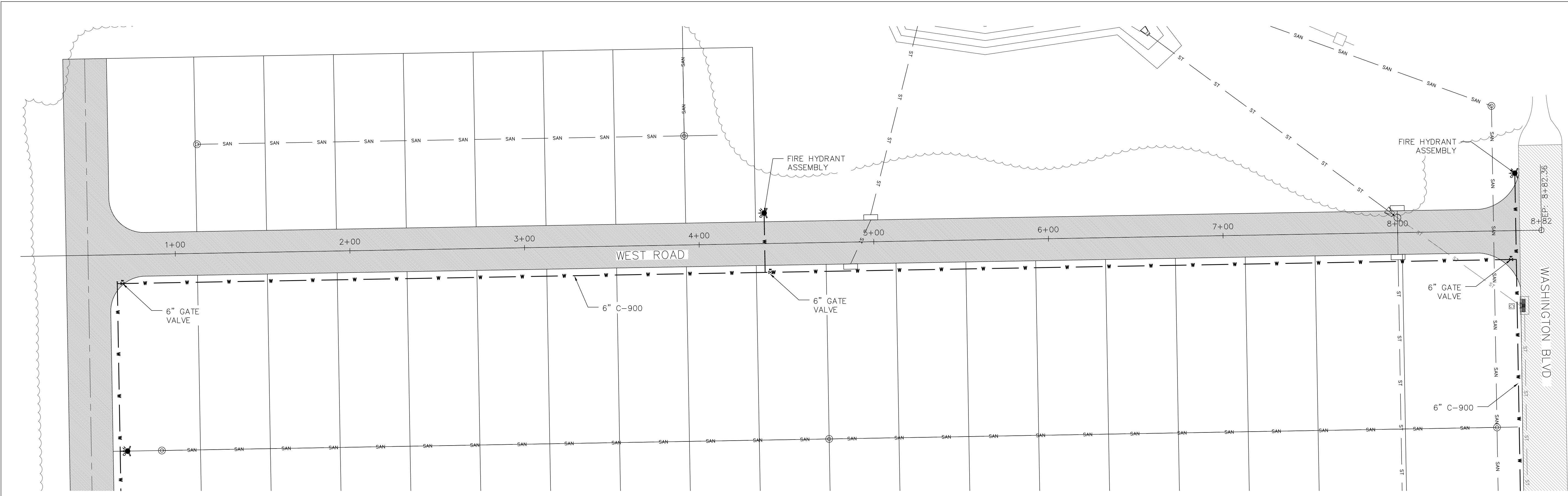
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WEST BRANCH VILLAGE, LLC
WEST BRANCH VILLAGE EXPANSION
WEST BRANCH, IOWA

WATER MAIN PLANS
EAST ROAD

SHEET NO.
W.02



ORDINANCE 781

AN ORDINANCE AMENDING CHAPTER 165; ENTITLED, “ZONING REGULATIONS” TO ADD SECTION 51, “ANNEXED TERRITORY”.

WHEREAS, the City of West Branch, Iowa, believes that the clarification of certain sections in the Zoning Code will lead to a clearer understanding of the process of Annexation; and

WHEREAS, the City of West Branch, Iowa, believes that a more uniform and consistent wording in the language contained within certain sections in the Zoning Code will aide interested parties better navigable the state and local process of annexation; thus, lead to more consistent interpretation of the requirements; and

WHEREAS, it is the goal to make development in the City of West Branch user friendly; thus, it is recommended that the City Council of the City of West Branch, adopt these amendment recommendations in the Zoning code; and

WHEREAS, the City Council of the City of West Branch, Iowa has reviewed these recommendations and concurs with the intention of providing a framework for proposals for annexed territory.

NOW, THEREFORE, BE IT ORDAINED:

BE IT ENACTED by the City Council of West Branch, Iowa, that Chapter 165 “ZONING REGULATIONS” of the Code of West Branch, Iowa is hereby amended by revising the following subsections to Chapter 165.

165.51 – Amend Chapter 165 by adding section 51: ANNEXED TERRITORY

165.51 ANNEXED TERRITORY

- 1. Annexation is defined as the addition or incorporation of a land / territory (private and/or commercial) into the City of West Branch.**
- 2. Any area of proposed annexation to the City of West Branch shall be zoned by the City after a recommendation by the Planning and Zoning Commission prior to annexation. Any applicant for annexation must include a requested zoning classification along with the application for annexation. The Notice of Annexation must include the applicant’s requested zoning classification. If the Planning and Zoning Commission makes a recommendation for acceptance of the annexation to the Council, it must include a recommendation of zoning also. If the Council accepts the annexation, it must designate the zoning classification of the territory and it will become effective upon the recording of the resolution. The resolution will not be recorded sooner than three days after its passage, nor will the resolution be sent to the State of Iowa City**

Development Board until three days after passage. The applicant for annexation may withdraw the application if desired due to the zoning classification within the three day period before the annexation is recorded or forwarded.

3. The ability to annex and approval of an annexation proposal are subject to section 368 (25, 0) of the Iowa Code.

Passed and approved this ____ day of _____, 2021.

First Reading: May 17, 2021

Second Reading: June 7, 2021

Third Reading: June 28, 2021

Roger Laughlin, Mayor

Attest:

Redmond Jones, City Administrator/Clerk

January 2019

NOTIFICATION to Iowa Cities and Counties

SUBJECT: Permit Process for Advertising Signs

The purpose of this letter is to inform you of the Department's responsibility to control the erection and maintenance of advertising signs along interstate and primary highways.

We have found that some local authorities have been unaware of state regulations that must be met in addition to local ordinances with respect to advertising signs on private property, (no signs, other than those authorized by the Manual on Traffic Control Devices are allowed in the right of way), along interstate and primary highways.

Signs advertising a product, which is not available, or an activity that does not take place at the sign's location are considered off-premise signs and outdoor advertising permits are required from the Department. A sign must meet established size, lighting, spacing, and zoning requirements before a permit can be issued.

We ask that in your dealings with sign owners that you alert them to the possibility that a state permit might be necessary. This will eliminate any unnecessary expense that could occur if they erect a sign that conforms to the local ordinance but must be removed later because it does not comply with state law.

We appreciate your cooperation in this matter. If you are not the person who oversees the signing ordinances for your jurisdiction, we would ask that you pass this letter on to the appropriate individual.

We do, as a matter of practice, require that the local zoning authority sign off on all outdoor advertising permit applications. This gives you the opportunity to share with the sign owner any regulations and requirements that you might have in addition to those contained in state law. This does not prohibit the local authority from establishing standards, which are more restrictive than state law. It is, however, the responsibility of the local jurisdiction to enforce their own ordinances.

If you have any questions regarding this matter feel free to contact our office at the telephone number listed above or visit our website at www.iowadot.gov/iowaroadsigns.

Sincerely,

Brent Christian
Right of Way Agent
brent.christian@iowadot.us

BC:sa



6. No off-street parking shall be permitted in the grass area in the required maximum front yard of any residential district.
7. Off-street parking spaces provided on other than the same property as the use is located shall be permitted only in such district permitting parking as a use. Such separate parking spaces shall be maintained as long as the principal building or uses are maintained.

165.40 ACCESS DRIVES.

1. All off-street parking facilities shall be designed with appropriate means of vehicular access to a road or alley in a manner which will least interfere with traffic movements.
2. Access to a State Highway shall be subject to the regulations of the Iowa State Highway Commission. The administrative officer shall consider access location and proximity of intersections before granting permits, and shall refer the factors to the Highway Commission for approval if required.

165.41 OFF-STREET LOADING. Off-street loading and unloading space with proper access from a street or alley, and with at least fourteen (14) feet of vertical clearance shall be provided, either within or outside the building to adequately serve the use on the lot. All off-street loading and unloading spaces shall have an all-weather surface to provide safe and convenient access and use during all seasons.

165.42 HEIGHT LIMITATIONS. In Residence Districts, no building shall exceed thirty-five (35) feet in height, provided that such height limits may be exceeded by one (1) foot for each foot by which the width of each side yard is increased beyond minimum side yard requirements, up to a maximum of fifty (50) feet. In commercial and industrial districts, no building shall exceed fifty (50) feet in height, provided that such height limits may be exceeded when authorized by the Board of Adjustment. Structures supporting utility facilities are exempted from the provisions of this section.

165.43 SIGNS.

1. Permitted Signs, R District:
 - A. Nameplates and Identification Signs, subject to the following:
 - (1) For one- and two-family dwellings, there shall be not more than one nameplate, not exceeding one square

foot in area, for each dwelling unit, indicating the name or address of the occupant or a permitted occupation.

(2) For multiple-family dwellings, for apartments, hotels, and for buildings other than dwellings, a single identification sign not exceeding twelve (12) square feet in area and indicating only the name and address of the building and the name of the management thereof may be displayed.

B. “For Sale” and “To Rent” Signs, subject to the following:

(1) There shall be not more than one such sign per lot except that on a corner lot two (2) signs, one facing each street, shall be permitted. No sign shall exceed eight (8) square feet in area or be closer than eight (8) feet to any other zoning lot.

(2) No sign shall project beyond the property line into the public way.

C. Church Bulletins, subject to the following:

(1) There shall be not more than one sign per lot except that on a corner lot, two (2) signs, one facing each street, shall be permitted, if two signs are being installed neither sign shall exceed eighteen (18) square feet in area. If only one sign is installed on any lot no sign shall exceed twenty-five (25) square feet in area or be closer than eight (8) feet to any other zoning lot.

(2) No sign shall project beyond the property line into the public way.

(Ord. 589 – Sep. 05 Supp.)

2. Permitted Signs, B and I Districts:

A. All signs permitted in the residential district.

B. Signs on Marquees, Canopies and Awnings: Restrictions imposed herein on the projection of signs across property lines into the public way shall not apply to signs located on marquees or canopies, provided that any sign located on a marquee or canopy shall be affixed flat to the surface thereof, and, further, that no sign shall extend vertically or horizontally beyond the limits of said marquee or canopy, except that individual freestanding letters may project to a height not exceeding twenty-four (24) inches above same.

C. Signs on Pylons, Standards, Clocks and Supports: Signs, clocks or other advertising devices erected upon standards or separated supports shall be placed so as to be entirely within the property lines of the premises upon which they are located and no part of the sign or standard shall have a total height greater than thirty (30) feet above the adjoining ground level if such ground level is above the street level, nor shall the surface of any such sign exceed an area of one hundred (100) square feet.

D. Signs on Masonry Pylons. Signs may be placed on the face of a masonry pylon when the pylon does not project above the roof line more than twelve (12) feet and the type, design and construction of the pylon complies with all the requirements herein.

E. Billboards and poster panels having a sign area not exceeding four hundred (400) square feet, provided the location of their sites and the limitations of the time of their use, and all other terms and conditions thereof, are first approved by the Council.

F. Sign Requirements in Business Districts. All signs in business districts shall meet the following requirements.

(1) Area: The gross area in square feet of all signs on a zoning lot shall not exceed three (3) times the lineal feet of frontage of such zoning lot. The gross area of all flashing signs shall not exceed two (2) times the lineal frontage of such lot.

(2) Location: The sign or signs shall front the principal street, a parking area, or in the case of a corner building, on that portion of the side street wall within fifty (50) feet of the principal street.

(3) Projection: Signs suspended from any building shall not project more than seventy-two (72) inches beyond the building line but in no case project closer than twenty-four (24) inches to the curb line. The bottom of such sign shall not be less than ten (10) feet above the finished grade of the sidewalk. No sign except those suspended from buildings shall be erected, be placed or encroach upon the street property.

(4) B-1 Business District: Signs must not be illuminated so as to shine on residential properties.

Illumination shall be non-flashing and shall not contain a rotating, oscillating, revolving beam or beacon of light.

(5) B-2 Business District: One free standing sign shall be allowed if its illumination is non-flashing and does not contain a rotating, oscillating, revolving beam or beacon of light and may be installed at the property line.

G. Highway directional signs and markers which shall be made and installed in accordance with the specifications of the City for announcing the location of, or directing traffic to, given locations which include, but are not limited to, the following:

(1) Service areas, automobile, food, lodging.

(2) Business or business districts. Traffic or directional signs designating entrances, exits and conditions of use of parking facilities accessory to the main use of the premises may be maintained provided they are located within the property lines of the subject lot. Fences may be erected along the boundaries of a lot or yard, but no fence shall be constructed closer than eighteen (18) inches to other fences or other structures.

3. Permitted Signs, CB-1, CB-2 and CI-2 Districts. Signs in the CB-1, CB-2 and CI-2 Districts are subject to the provisions of the standards for signage design and display referred to in Chapter 26 (West Branch Preservation Commission) and on file in the City Hall.

4. Permitted Signs, A-1 District. Billboards and poster panels having a sign area not exceeding one thousand six hundred (1,600) square feet, provided the location of their sites and the limitations of the time of their use, and all other terms and conditions thereof are first approved by the Council.

(Ord. 512 – Sep. 99 Supp.)

5. Abandoned/Obsolete Sign Removal. It shall be the responsibility of the land/property owner to remove any sign or signs on premises where the associated use of the sign or signs for advertisement of an activity, business, product or service has been discontinued. Signs shall be removed within ninety (90) days of discontinued use.

(Ord. 528 – Sep. 00 Supp.)

165.44 FENCES / HEDGES / WALLS / RETAINING WALLS. Fences and hedges located within a front, side or rear yard or within five (5) feet of a lot line shall be subject to the following location, height, and building permit requirements.

IOWA ROAD SIGNS

ON-PREMISE SIGNS

ON-PREMISE SIGNS

Types of on-premise signs include business, real estate, personal opinion, political and crop variety signs.

Advertising device advertising the sale or lease of, or activities being conducted upon, the property where the sign is located. On-premise signs located along interstates have more restrictive regulations and may have to be permitted. If you have questions about an on-premise sign along the interstate, contact an Iowa DOT right-of-way agent at 515-239-1255.

The criteria to be used to determine if an advertising device qualifies as on-premises signing, excluding development directory signing, include, but are not limited to, the following.



1. A sign that consists solely of the name of the establishment or identifies the establishment's principal or accessory products or services offered on the property is an on-premises sign.
2. An on-premises sign must be located on the same property as the advertised activity or the same property advertised for sale or lease. A subdivided property is considered to be one property if all lots remain under common ownership and all lots share a common, private access to public roads. However, if any lot in the subdivided property is sold or disposed of in any manner, that lot will be considered to be separate property.
3. Contiguous lots or parcels of land combined for development purposes are considered to be one property for outdoor advertising control purposes provided they are owned or leased by the same party or parties. However, land held by lease or

4. An on-premises sign shall not be located on a narrow strip of land that cannot reasonably be used for a purpose related to the advertised activity other than signing.
5. An on-premises sign is limited to advertising the property's sale or lease, or identifying the activities located on or products or services available on the property.
6. An advertising device is not an on-premises sign if it consists principally of brand or trade name advertising and either the product or service advertised is only incidental to the establishment's principal products or services or the advertising brings rental income to the property owner. "Principally" means 50 percent or more of the display area of the sign.
7. An on-premises sign concerning the sale or lease of property shall not display the legend "sold" or "leased" or a similar message.

REAL ESTATE SIGNS



Placement of real estate signs by realtors, brokers, developers, financiers and others, including homeowners selling their own property, is subject to Iowa's regulatory sign laws and rules and in instances where the signs are visible from a state highway. Other local ordinances may also apply to signs placed along county roads and city streets.

View [frequently asked questions](#) about real estate signs.

PERSONAL OPINION SIGNS

First Amendment

The First Amendment to the U.S. Constitution guarantees its citizens certain rights, including the right of free speech. If a landowner or resident wishes to express their opinion on a matter, they may do so through a message on a sign posted on property they own or on property where they reside.

Limitations

Personal opinions may not be expressed through the posting of signs on the public right-of-way. Personal opinion signs should not obstruct the view for motorists at intersections or railroad crossings. The signs should not resemble or imitate official traffic control devices. Lighting should

IOWA ROAD SIGNS

BILLBOARDS

BILLBOARDS

Outdoor advertising devices that display products or services available at locations other than at the sign site are commonly referred to as "billboards." They are also called "off-premise signs".

For in-depth information concerning billboards, view the [Iowa Administrative Rules](#) and [Code of Iowa 306C](#).

Spacing requirements for billboards along [Iowa interstates](#), [primary freeways](#), and [primary highways](#).

Billboards are not allowed to be erected along Scenic Byways. [Check to see if the area you are inquiring about is on a Scenic Byway](#).

View a [detailed Iowa map of zoned counties](#) throughout Iowa.

[Billboard application](#)

[Billboard Guide for Non-Interstate Highways](#)

[Billboard Guide for Interstates](#)

CONTACT US

If you have any questions regarding billboards, please contact Iowa DOT's agent [Brent Christian](#) at 515-239-1673.



View a complete list of billboard [frequently asked questions](#) (and their answers).



Example 1: A permit is required for any sign that advertises a product or activity that does not take place on the same property as the sign.

Example 2: Many Billboards are owned by advertising companies who lease space to individuals or businesses. Length of leases are determined on an individual basis. Cut-outs and extensions are sometimes used for greater effect.

Example 3: Billboards are also known as "off-premise" signs.

CHAPTER 9:

TRANSPORTATION

Introduction.

The City of West Branch's transportation system is vital to everyday life within the city and for access throughout the region and beyond. Locally, the transportation system facilitates the movement of people as well as goods. The system allows residents to get from their homes to employment, education, medical care, recreation, and shopping opportunities. Good transportation allows people and goods to move efficiently and reliably to and from the city. Connections to regional transportation networks allow businesses to import and export goods quickly and efficiently, give access to the city by tourists, and allow the region to compete in the global marketplace. Along with the benefits from transportation, come unintended negative impacts. If left unchecked, pollution, noise, congestion, safety, and high maintenance costs can diminish their quality of life for local residents. In addition, some segments of the population such as the disabled, the elderly, and low-income persons are not able to utilize portions of the transportation system. Through this Comprehensive Plan, the City of West Branch will strive to provide efficient and affordable transportation to residents and businesses while also striving to minimize negative impacts. The City of West Branch will work to provide diverse and interconnected modes of transportation, accessibility, safety, and improved environmental quality.

Goal 1: To provide efficient and affordable transportation for city residents and businesses, while striving to minimize negative effects.

Objective 1: The City Council adopt the Complete Streets section in this chapter as city policy, and, in particular, ensure that all future street projects on new or existing streets encompass complete streets practices unless explicitly excluded by action of the City Council.

Objective 2: The City should work to encourage city residents and/or staff to become members of RPA 10 committees to encourage full

consideration of West Branch projects in future RP 10 transportation plans and TIPS.

Objective 3: The city will ensure that adequate pedestrian facilities, bicycle parking and vehicle parking for the disabled are pursued as elements of its complete streets policies.

Objective 4: West Branch should continue to work with state and regional transportation staff to improve safety for all its road users.

Goal 2: It is City policy to increase children's physical activity to benefit their short- and long-term health and improve their ability to learn.

Objective 1: Children are to be given safe and appealing opportunities for walking and bicycling to school and to other access points such as the public library, in order to decrease rush-hour traffic and fossil fuel consumption, encourage exercise and healthy living habits, and reduce the risk of injury to children through traffic collisions near schools.

Objective 2: The City will partner with the West Branch School District to encourage programs such as Walk and Bike to School Days, as well as "Walking School Bus"/ "Bike Train" programs at elementary schools (where parents take turns accompanying a group of children to school on foot or via bicycle). Both parties will encourage educational programs that teach students safe walking and bicycling behaviors and educate parents and drivers in the community about the importance of safe driving.

Goal 3: Trails as both transportation and recreation facilities should be planned and implemented within West Branch and as connections to surrounding areas.

Objective 1: The City Council should adopt a trails vision and support the production of a map indicating desired trail connections.

Objective 2: West Branch should plan a trails system to serve transport and recreation users, and seek financing to implement the system.

Goal 4: The city should monitor changing transportation circumstances, such as evolving technology, to ensure the city has adequately and efficiently provided transportation for its future needs.

Objective 1: Publicize and promote paratransit services. Strive for cost effectiveness in service provision, perhaps enlisting volunteer support.

Objective 2: Promote low-cost and environmentally friendly service that is provided by employers, such as the University of Iowa vanpool, is a city objective.

Objective 3: Be attentive to the needs for freight service and ensure trucking needs are accounted for in street planning, design, and administration.

Objective 4: Keep abreast of changes in transport services and technology that impact the city, and in particular prepare for network service operations and the emergence of electric and autonomous vehicles.

Objective 5: The city should consider adding public electric vehicle charging stations and encouraging home charging stations for new construction or reconstruction.

Transportation Planning.

The City of West Branch coordinates with a regional transportation planning agency, Regional Planning Affiliation 10 (RPA 10—staffed by the East Central Iowa Council of Governments). RPA 10 is a seven-county region that includes Benton, Cedar, Iowa, Johnson, Jones, Linn, and Washington counties. RPA 10 was first formed in 1964 and facilitates coordination among local, regional, state, and federal agencies on transportation issues and plans. RPA 10 is responsible for planning for the development of a seamless system for the safe and efficient movement of people and goods within and between modes of transportation including streets, highways, bicycle, pedestrian, transit, rail, water, and air. RPA 10 has a 14-member policy committee with two members from each affiliated county and a technical advisory committee with three members from each county; in 2019 none of the Cedar County members on either committee were from West Branch.¹

The City of West Branch's Comprehensive Plan provides an overview of the City's transportation system. More detailed transportation information is included in the RPA 10 Long Range Transportation Plan (LRTP) for passenger transport. The RPA 10 LRTP focuses on transportation-related issues over a 20-year period. In accordance with Iowa DOT recommendations, RPA 10 updates its LRTPs every five years. The LRTP provides an explanation of the area's current transportation trends, and a forecast of future transportation issues. Through the LRTP planning process, local residents create the vision and goals that will guide transportation investment within the region over the next 20 years. The RPA 10 LRTP and other transportation planning documents (such as a trails plan) are available for download at www.ecicog.org, under plans. The plan that was current in 2019 covers 2016-2020, and the current Transportation Improvement Program (TIP), adopted in 2018, covers 2019-2022. That TIP lists one bridge replacement project in West Branch, over Wapsinonic Creek. The City desirably should work to encourage city residents and/or staff to become members of RPA 10 committees to encourage full consideration of West Branch projects in future plans and TIPS.

Roadways.

The predominant transportation system in West Branch is a network of streets and highways that carry cars and trucks, but also can be utilized by bicyclists. These roadways serve the circulation needs of local residents and those of visitors from outside the area. The following section describes the roadway system connecting the City in terms of functional classification, capacity, congestion, and safety. Following that section is information on complete streets and sections about alternative forms of transportation and intercity transportation connections.

Functional Classification.

Functional classification is commonly used to describe roadways based on the type of service that they provide. Roadways provide two basic types of service: land access, and mobility. The degree to which a roadway provides access and/or mobility determines its functional classification. The key to planning an efficient roadway system is finding the appropriate balance between mobility and accessibility. The following section describes functional classifications. Map 9.1 displays West Branch roads by Functional Classification.

Principal Arterial roadways primarily serve a mobility function with minimal land access. The primary purpose of principal arterials is the relatively rapid movement of people and goods for extended distances. Principal arterials are higher-capacity, higher-speed roadways with restricted land access. Although not meeting all those criteria, the closest roads to principal arterials in West Branch are Main Street (and its extension out of town as Hoover Highway), Parkside Drive, and Baker Avenue. An important highway for passenger and freight transportation that serves the function of a principal arterial borders West Branch to the south, Interstate Highway 80. I-80 is expected to increase its traffic over the span of this comprehensive plan, particularly its heavy truck traffic.² A capacity expansion for I-80 from four to six lanes by the Iowa Department of Transportation was in the planning process in 2019, together with reconstruction of the I-80 interchange at Hoover Highway.

Minor Arterials interconnect with and augment principal arterials. Minor arterials within urban areas serve inter-community trips of moderate length. Although the primary purpose of the minor arterial is mobility, this functional class provides more access points and more land access than a principal arterial. An example is Johnson-Cedar Road in West Branch. Main Street in West Branch might be described as a minor arterial due to its low speeds and lack of access control.

Collector streets channel trips between the local street system and the arterials. An example is Orange Street. Collectors serve a balance between mobility and land access. Parking and direct driveway access to the street are typically allowed on collectors. Collectors are usually wider, have higher capacity, may have some parking restrictions, and permit somewhat higher speeds than the local street network.

Local Streets primarily provide local land access and offer the lowest level of mobility. Characteristics of local streets include uncontrolled intersections, posted speed limits of 25 miles per hour or less, and few restrictions on parking. Local streets include all streets not classified as principal arterial, minor arterial, or collector.

Traffic Volume.

Traffic volume is an important measure for the transportation system. Understanding traffic volume helps engineers and transportation planners design a road system that is appropriate for the community. Traffic volume is measured in Average Annual Daily Trips (AADT). AADT is the total traffic volume on a road for a year divided by 365 days. Map 9.2 displays the AADT for West Branch area roads. The data used in Map 2 was collected by the Iowa DOT in 2014 (and was the latest information available at https://iowadot.gov/maps/Digital-maps/traffic/city_trafficmaps in May, 2019). Main Street and Parkside Drive were the most heavily traveled West Branch city streets in 2014 (the most recent year for which the Iowa DOT had data in 2019) with 4,610 and 3,980 AADT respectively. Interstate 80 had the highest traffic volume in the area with an AADT of 34,400.

Roadway Safety.

Roadway safety is an important consideration when planning for the future of a transportation system. Outdated or deteriorating infrastructure, high traffic volumes, or unsafe driver behavior are all potential causes of safety issues that can lead to serious injury or death. Transportation planners use crash data to identify areas on the road network where the number of crashes is higher than expected. Once identified, a local government may take action to correct the problem. The Iowa Department of Transportation provides crash data for all counties in the state at but unfortunately this site shows data only for larger Iowa cities (5,000 population and above). However, rural Cedar County data for 2007-2011 are available at https://iowadot.gov/crashanalysis/data/county/general/_counts/counts20072011/images/jpegs/cedar20072011.jpg.

For the 2013 plan, using DOT data from 2008-2010, ECICOG staff created a map to illustrate the distribution of crashes. Map 9.3, which is included in this chapter from the 2013 plan, identified locations of crashes for that period. During the three-year time period 43 crashes occurred within city limits. The crash data suggest that while the City

is relatively safe, there are some problem areas. Injuries and fatalities from auto crashes can place a burden on local residents and can reduce the overall quality of life in the City. By observation in 2019, it is to be noted that Hoover Highway, continued to Main Street, has been experiencing increased bicycle traffic between the Iowa City area and West Branch. Limited sight distances and minimal shoulders on Hoover Highway suggest potential conflict with motor vehicles and an emerging safety problem. West Branch should continue to work with state and regional transportation staff to improve safety on Hoover Highway and throughout the city.

Complete Streets.

While functional classification may be helpful in describing a city's roadways, transportation planners and many localities increasingly find an additional concept more useful when describing and implementing the role of their street networks. This concept is termed "complete streets," and a policy of promoting complete streets was first discussed on 5/24/16 by the Planning & Zoning Commission, then further discussed and the following vision statement and policy approved by the Planning and Zoning Commission in 2017. Such a policy requires that design and implementation practices for city streets follow a policy of careful and individualized evaluation of street construction, reconstruction and maintenance procedures using current design options that comply with state and federal requirements in the following manuals, as well as others when feasible, to ensure access for all users:

- AASHTO Green Book: A Policy on Geometric Design of Highways and Streets, by the American Association of State Highway and Transportation Officials;
- AASHTO Guide for the Development of Bicycle Facilities, by the American Association of State Highway and Transportation Officials;
- Urban Street Design Guide, by the National Association of City Transportation Officials;
- Urban Bikeway Design Guide, by the National Association of City Transportation Officials and the Iowa Statewide Urban Design Standards, by the Institute for Transportation at Iowa State University.

As necessary, West Branch street ordinances should be revised to accord with complete streets policies.

The Commission's complete streets vision and policy statement follow.

Nearby communities and the MPO of Johnson County also incorporate the concept in their plans and ordinances.³

It is proposed that the Commission recommend that the City Council adopt this Complete Streets section as city policy, and, in particular, ensure that all future street projects on new or existing streets encompass complete streets practices unless explicitly excluded by action of the City Council.

Transportation Vision Statement

West Branch envisions a transportation system that encourages healthy, active living; promotes transportation options and independent mobility; increases community safety and access to healthy food; reduces environmental impact; mitigates climate change; and supports greater social interaction and community identity by providing safe and convenient travel along and across streets through a comprehensive, integrated transportation network for pedestrians, bicyclists, public transportation riders and drivers, motor-vehicle drivers, emergency vehicles, freight, and people of all ages and abilities, including children, youth, families, older adults, and individuals with disabilities.

Complete Streets Policy

It is the policy of the city of West Branch to provide safe and comfortable routes for walking, bicycling, and, in the future when practicable, public transportation, to increase the use of these modes of transportation, enable convenient and active travel as part of daily activities, reduce pollution, and meet the needs of all users of the streets, including children, families, older adults, and people with disabilities.

The City will integrate complete streets infrastructure and design features into street design and construction to create safe and inviting environments for all users to walk, bicycle, and use public transportation.

In planning, designing, and constructing complete streets: to the greatest extent practicable West Branch will include infrastructure that promotes a safe means of travel for all users along the right-of-way, such as sidewalks, shared-use paths, bicycle lanes, and paved shoulders.

West Branch will include infrastructure that facilitates safe crossing of the right-of-way, such as accessible curb ramps, crosswalks, refuge islands, and pedestrian signals; such infrastructure must meet the needs of people with different types of disabilities and people of all ages. The City will ensure that sidewalks, crosswalks, and other aspects of the transportation right-of-way are compliant with the Americans with Disabilities Act and meet the needs of people with different types of disabilities, including mobility impairments, vision impairments, hearing impairments, and others.

West Branch will consider incorporating street design features and techniques that promote safe and comfortable travel by pedestrians, bicyclists, and public transportation riders, such as traffic-calming circles, additional traffic-calming mechanisms, narrow vehicle lanes, raised medians, road diets, high street connectivity, and physical buffers and separations between vehicular traffic and other users.

To the extent practicable, West Branch will provide pedestrian-oriented signs, pedestrian-scale lighting, benches and other street furniture, and bicycle parking facilities. The city will encourage street trees, landscaping, and planting strips, including native plants where possible, in order to buffer traffic noise and protect and shade pedestrians and bicyclists. The City's long-term goal is to reduce surface water runoff by reducing the amount of impervious surfaces on the streets. It is the City's intent to have all future street projects on new or existing streets encompass complete streets practices unless explicitly excluded by action of the City Council.

Transportation for Youth.

It is City policy to increase children's physical activity to benefit their short- and long-term health and improve their ability to learn. Children are to be given safe and appealing opportunities for walking and bicycling to school in order to decrease rush-hour traffic and fossil fuel consumption, encourage exercise and healthy living habits in children, and reduce the risk of injury to children through traffic collisions near schools.

The City will partner with the West Branch School District to pursue encourage programs such as Walk and Bike to School Days, as well as "Walking School Bus"/"Bike Train" programs at elementary schools, where parents take turns accompanying a group of children to school on foot or via bicycle. Both parties will encourage educational programs that teach students safe walking and bicycling behaviors, and educate parents and drivers in the community about the importance of safe driving.

Bicycle and Pedestrian Transportation.

Non-motorized transportation is a key component of a multi-modal transportation system. Good walking and biking facilities can improve quality of life by reducing the number of vehicles on the road, promoting an active lifestyle, attracting visitors to the area, and providing a low-cost mode of transportation. In recent years, the City of West Branch has worked to integrate bike and pedestrian facilities into its transportation network. However, according to 2010 Census estimates, only 3.2% of the population walks to work, and only 0.47% rides a bike.⁴

West Branch's bike and pedestrian facilities fall into two categories, separated and on-street facilities. A separated facility is a bikeway/walkway physically separated from motorized traffic by open space or barrier either in the highway right-of-way or in an independent right-of-way. Separated facilities are suitable for all pedestrians and bicyclists. In an on-street route, bicyclists share space with motorized vehicles. On-street routes can take several forms including bike lanes or shared roadways. In some cases, striping, signing, and pavement markings designate a portion of the roadway for the preferential or exclusive use of bicyclists. In other cases, an on-street route signage indicates that the route is safe for bicyclists.

When planning a bicycling, hiking, and walking system, local governments should design a system that will accommodate as many users as possible in accord with complete streets policies. One interesting possibility for West Branch's system would be a bike-sharing program such as recently instituted by Solon, Iowa.⁵ Such a program could help expand bicycle usage by city residents, but could

⁴ U.S. Census Bureau, 2006-2010 American Community Survey.

http://factfinder2.census.gov/faces/tableservices/jsf/pages/productview.xhtml?pid=ACS_10_5YR_B08301&prodType=table.

⁵ For a description of Solon's program and its funding see: <https://patch.com/ohio/solon/solon-bicycle-renting-program-gets-boost-grant>.

be an effective way to bring visitors from the Hoover complex to historic properties and commercial activity in West Branch.

West Branch's system should take into consideration the differing abilities of the potential riders using the system. The Federal Highway Administration (FHWA) uses the following categories of bicycle users to assist in determining the impact that different facilities and roadway conditions will have on the bicyclist. Group A riders have the most experience, and are comfortable riding on most city streets. Group B bicyclists are less experienced and prefer riding on separated trails or low speed low traffic volume streets. Group C bicyclists are children. Children often use bicycles to get to school or recreation, but require well-defined separation from motor vehicles.

The Bicycle Federation of America estimates that out of nearly 100 million people in the United States that own bicycles, roughly 5 percent qualify as Group A bicyclists, with the remaining 95 percent as Group B and C bicyclists. See Map 1.6 for the bike and pedestrian facilities in West Branch.

In this update to the comprehensive plan, the Commission adopts the following vision statement for trails. That statement was created at an October, 2017, West Branch Trail Meeting, chaired by Mayor Roger Laughlin, and attended by interested West Branch citizens and city staff.

Trails Vision Statement.

West Branch will have a trail system linking city parks, schools, National Park Service, West Branch Public Library, business districts, neighborhoods and other communities, enhancing the lives of residents, and attracting more visitors to enjoy our community.

At the 2017 meeting, it was proposed that elements of the system should be as follows.

1. Verbal Statement of destinations to be connected.
 - a. West Branch Public Schools, West Branch Public Library, NPS, Business District, Beranek Park, Wapsi Park, Cubby Park, West Branch Dog Park and Hoover Nature Trail were all discussed as

destinations within the city limits.

- b. There is a desire to connect the West Branch trail system to other communities such as West Liberty, Iowa City, Solon, and Tipton.
2. List of existing trails and desired trail connections not yet built.
 - a. Beranek Park Connection
 - b. Cubby Park Connection
 - c. Improve sidewalk long north side of Main Street creating a connection to Hoover Park at Parkside Drive
 - d. Trail through the Meadows to Greenview
 - e. Eventually asphalt Hoover Nature Trail.
3. Map of the items mentioned above.

Superintendent Pete Swisher will check with the NPS resources and see if they can assist with the map. Mayor Laughlin will contact Cedar County GPS department and determine if they will be able to assist with mapping. *Map to be forthcoming.*
4. List of near-term priorities
5. General discussion of funding sources.

The 2011 City Trails Plan has a list of potential trail funding sources. An update to that Trails Plan that envisions 9 miles added to the current 4 miles of streets, sidewalks, and trails was presented to the City Council in June, 2019, although the update did not include trail funding.⁶

Safety – Improving bicycle and pedestrian safety will be a primary concern for West Branch. According to Iowa DOT, the state averages 5 bicyclist and 21 pedestrian fatalities each year. In West Branch, City injury hospitalization data shows bicycle and pedestrian accidents as one of the leading causes of injury and death for residents between the ages of 1 and 34. The Iowa DOT recommends the following to improve bicycle and pedestrian safety:

- Young children need supervision in the traffic environment.
- Children should learn bicycle and pedestrian safety from an early age.
- Wearing a helmet can reduce the risk of head injury by as much as 85%.⁷

⁶ See <http://www.westbranchtimes.com/article.php?id=15884>.

⁷ Iowa Department of Public Health. 2002-2006 The University of Iowa Injury Prevention Research Center. <http://www.public-health.uiowa.edu/iprc/resources/reports/Cedar-County.pdf>.

Distance – West Branch’s semi-rural character means that walking or bicycling to a destination can be difficult because of the distances involved. Local governments can help reduce travel distances by encouraging compact development that reduces sprawl and promotes land-use patterns that create more walkable neighborhoods. Examples of this include conservation subdivisions, mixed-use development, cluster development, planned unit developments, and infill development.

Infrastructure – Incomplete infrastructure prevents many West Branch residents from walking and biking. Local governments can fill the gaps in the bicycle and pedestrian network working to obtain funding for trails and other facilities, and by adopting new sidewalk and Complete Streets policies. Complete Streets are designed to allow pedestrians, bicyclists, and transit to travel safely alongside automobiles. West Branch communities should continue to work to improve the quality of its residents by supporting programs that make walking and biking safer and more convenient. West Branch should also collaborate with Cedar County, Johnson County and surrounding communities to improve trail infrastructure, both for the use of city residents and to economically support local businesses through tourism.

Transit.

East Central Iowa (ECI) Transit provides demand-responsive rural transit service in the counties of Benton, Iowa, Johnson, Jones, Linn, and Washington counties. ECICOG is responsible for administration, coordination, and planning of ECI Transit. ECICOG does not directly operate the transit service. ECICOG contracts with a transit service provider in each affiliated county. In Cedar County, River Bend Transit (RBT) provides transit services to persons with disabilities, the elderly, and the general public. According to RBT’s website, “RBT was Iowa’s first regional consolidated transit system, starting public transit operations in 1978.” In Johnson County, transit services are provided by Johnson County SEATS.⁸ (see <https://riverbendtransit.org/about>).

There are a few rural transit services that operate primarily using volunteers. One such example is OATS Transit in Missouri (see <https://www.oatstransit.org/annual-report>). Although such a volunteer-supported service does not appear likely for West Branch,

investigating prospects for greater use of volunteers to provide mobility should be kept in mind over the planning period.

While West Branch is not expected to have a sufficiently large population to support conventional bus transit services over the period of this comprehensive plan, a few city residents do currently participate in paratransit services through a University of Iowa (UI) vanpool program. According to a representative of the UI’s Parking and Transportation Division, “Over the years we have had three (3) vanpools from West Branch. One maxi-van and two mini-vans. The maxi-van was the first to start and is the only one that is still operating.” That current maxi-van operates at 7:30 AM daily from West Branch, returning from Iowa City at 4 PM. Vanpool usage data for 1996 through early 2019 are available in a spreadsheet provided to city staff as part of preparing this plan update. Promoting such low-cost and environmentally friendly service that is provided by employers should be a city objective.

Taxicab and transportation network companies (such as Uber and Lyft) currently provide service to and from nearby communities. As technology develops, autonomous vehicles may provide mobility services for West Branch residents.

Freight.

The efficient movement of goods is one of the keys to effective competition in national and world markets. As a result, policy makers, industry specialists, and transportation planners have recognized that an efficient freight system is fundamental for economic development in cities such as West Branch. This section focuses on the three freight modes which are most active in the West Branch: truck, rail, and air. The freight modes are described separately, but the different modes are often used in combination, which is referred to as intermodal freight transport.

Interstate 80 passes through West Branch. This highway provides a ground connection to the rest of Iowa, the region, and the nation. Air freight service is available through the Cedar Rapids and Moline airports. The rail system that passes through the region may also be a

⁸ Current information on paratransit services and funding should be obtained as needed.

valuable but more limited resource. Iowa Interstate Railroad operates an east-west line that runs approximately 2.5 miles south of West Branch.

Business establishments need attention to freight pick-up and delivery services in order that they be efficient and reliable. Often a city needs to give attention to loading and unloading areas and to special regulations for truck activity. West Branch should be alert to freight needs and emerging freight transport technologies (such as those anticipated in the region by Google) as it plans, designs and administers its street facilities.

Airport.

The Eastern Iowa Airport in Cedar Rapids provides commercial air transportation services for West Branch. Recently thirty-five daily departures (with 14 non-stop destinations) have provided service from American, United, Delta Frontier and Allegiant Air. The Airport also features increasing cargo activity with Fed Ex, DHL, and UPS and serves an active general aviation community. The Airport is owned by the City of Cedar Rapids and operated by the Cedar Rapids Airport Commission. The Airport is located between Cedar Rapids and Iowa City along Interstate 380.⁹

The Quad City International Airport also provides commercial air transportation services for West Branch. The Quad City International Airport is located in Moline, Illinois and currently has four airlines serving 11 nonstop hubs or connecting cities.¹⁰ The airport is owned by Rock Island County and is operated by the Metropolitan Airport Authority. The airport also has cargo activity with Fed Ex, DHL and UPS. General aviation airports are located in Iowa City, Muscatine, and Tipton.

Intermodal Facility.

Railroads through their connections with other transportation modes are involved in many intermodal traffic movements. Rail typically provides the long-haul portion of the movement and at an intermodal facility the freight is transferred to another mode for door-to-door delivery. The closest rail facility to the City of West Branch is located in West Liberty, Iowa. The Iowa Interstate Railroad passes through West

Liberty although its intermodal service is currently available only in Council Bluffs and Blue Island, IL.¹¹

West Branch Intercity Passenger Transportation

Greyhound.

The closest Greyhound station to West Branch is in Iowa City. One bus leaves each day for Chicago with tickets costing \$43 per seat in 2019. One bus leaves each day for Omaha; tickets cost from \$79 to \$87.¹² Below is a map of the regional Greyhound routes from Iowa City:

Map 9.1. Regional Greyhound Routes From Iowa City



Source: Greyhound Corporation.

⁹ See <https://flycid.com/airlines-nonstops/>.

¹⁰ See <https://www.qcairport.com/airlines-serving-mli.html>.

¹¹ See <https://iaisrr.com/ship-with-iais/intermodal/>.

¹² See <https://www.greyhound.com/en/ecommerce/schedule/>.

Burlington Trailways.

The closest Trailways stations to West Branch are Iowa City and Cedar Rapids. Its intercity options have had the greatest numbers of destination and frequencies.¹³ The table below summarizes the recent prices and service frequencies from Cedar Rapids and Iowa City to Chicago, Des Moines and Minneapolis:

Table 9.1. Recent Prices and Service Frequencies

Origin	Destination	Price	# Daily
Cedar Rapids	Chicago	\$57	3
Cedar Rapids	Des Moines	\$34	1
Cedar Rapids	Minneapolis	\$99	1
Iowa City	Chicago	\$32	4
Iowa City	Des Moines	\$25	5
Iowa City	Minneapolis	\$80	2

Source: Unknown

Map 9.2. Routes for Trailways extend to Denver, St. Louis and Indianapolis.



Source: Burlington Trailways

¹³ See <https://webstore.trailways.com/buy-bus-tickets/ia>.

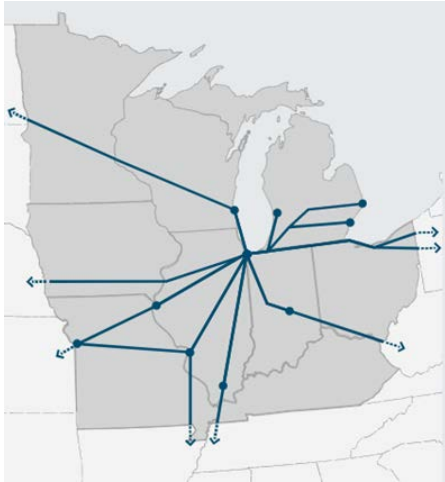
Megabus.

Megabus (Windstar Lines) is a newer carrier accessed in Coralville (at the Coralville Transit Intermodal Facility, 906 Quarry Rd., Coralville, near Iowa City) with daily service to Chicago, Des Moines, and Omaha. These buses have a variety of passenger amenities. Fares are variable.¹⁴

Passenger Train.

The closest train station is in Mt. Pleasant, where one can catch Amtrak's California Zephyr line. Amtrak travels daily east to Chicago (\$41) and west to Denver (\$108) and Oakland (\$203).¹⁵ Daily service is also newly available from Chicago to Moline with fares in 2019 starting at \$42.¹⁶ A regional map for Amtrak follows:

Map 9.3. Amtrak Regional Map



Source: Amtrak

Transportation Projects.

Transportation is extremely important to the citizens of West Branch. The primary transportation mode is vehicle travel on streets and roads. The public input survey found that the majority of residents supported the replacement of College Street Bridge, opened 11/21/19, followed by repaving 4th Street and repaving County Line (Johnson-Cedar) Road. Sidewalks were the second mode of transportation cited

by citizens as in need of attention. Residents would like to see sidewalks throughout the entire community, and identified the following areas for sidewalk improvement: College Street Bridge, College Street, Poplar and Oliphant, downtown, Cedar Street, Johnson Street, and pointed out the need to build missing sidewalks and address steps (elevations). City staff can provide a detailed map of street conditions in the City of West Branch. This map should be used in identifying future street projects within the City.

Another project identified as needed is a connection between Pedersen Valley and Greenview. When the land that had been in agriculture between Pedersen Valley and Greenview is developed into residential housing, the City Council should ensure that the developer provides a street connection between the two neighborhoods. This will allow for an improved public safety response time for residents in Greenview, and provide for better circulation of traffic in this area. A majority of survey responses collected on this issue supported a connection between the two neighborhoods. In 2019 it was anticipated that the extension of Orange Street in the near future would satisfy this need for connection.

Transportation Technology and Electric Vehicles.

At their joint meeting of October 24, 2017, the Commission and the City Council discussed emerging changes in the nation's vehicle fleet and the desirability of quickly responding to those anticipated changes, particularly to the emergence of greater numbers of electric vehicles. As a result, the Mayor agreed to explore with Alliant Energy and potential local partners the prospects for installation of electric charging stations in West Branch at an early date, and the publicizing of station locations as a city attraction. While his exploration was ultimately unsuccessful, as transport technology such as automated vehicles continues to evolve the city should keep abreast of new developments to track mobility opportunities and to determine their impact on city facilities.

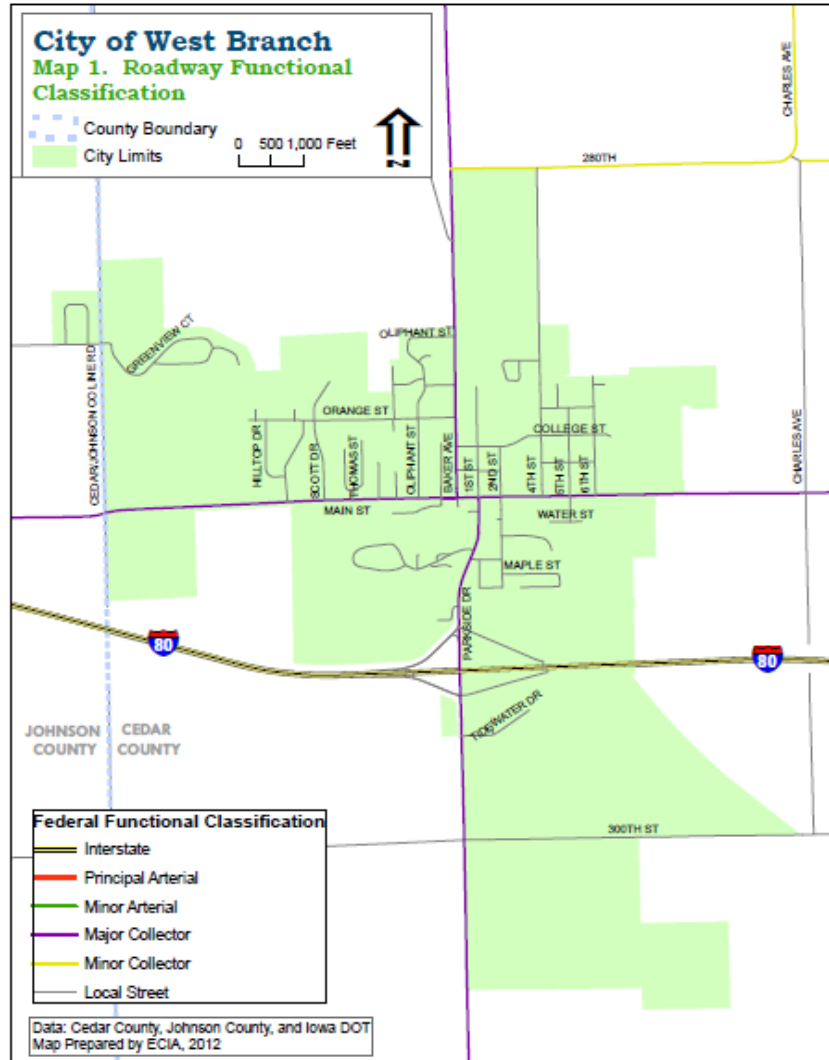
¹⁴ For information see <https://us.megabus.com/route-guides/des-moines-to-iowa-city-coralville-bus>.

¹⁵ See <https://iowadot.gov/iowarail/iowa-Passenger-Rail/Todays-Passenger-Rail> and

¹⁶ See <https://www.amtrak.com/stations/mli>. Also <https://www.amtrak.com/midwest-train-routes>.

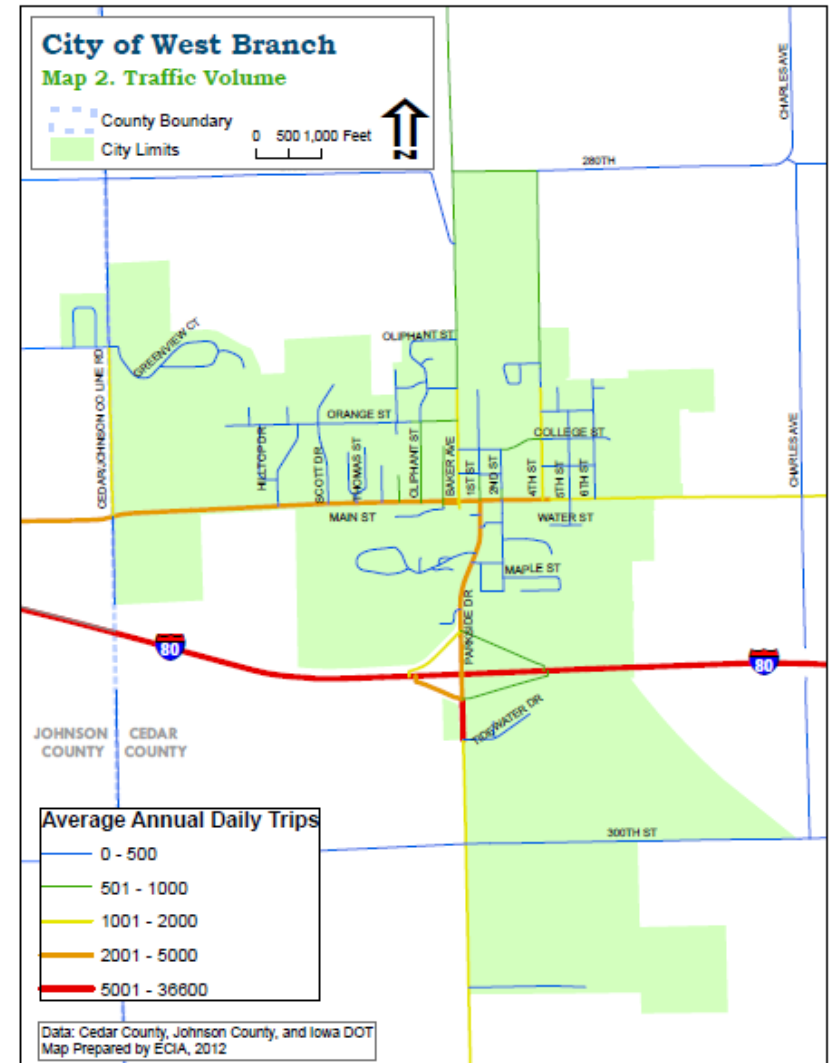
Service may eventually be extended to downtown Iowa City, but without intermediate stops in Iowa.

Map 9.4. West Branch Roads by Functional Classification



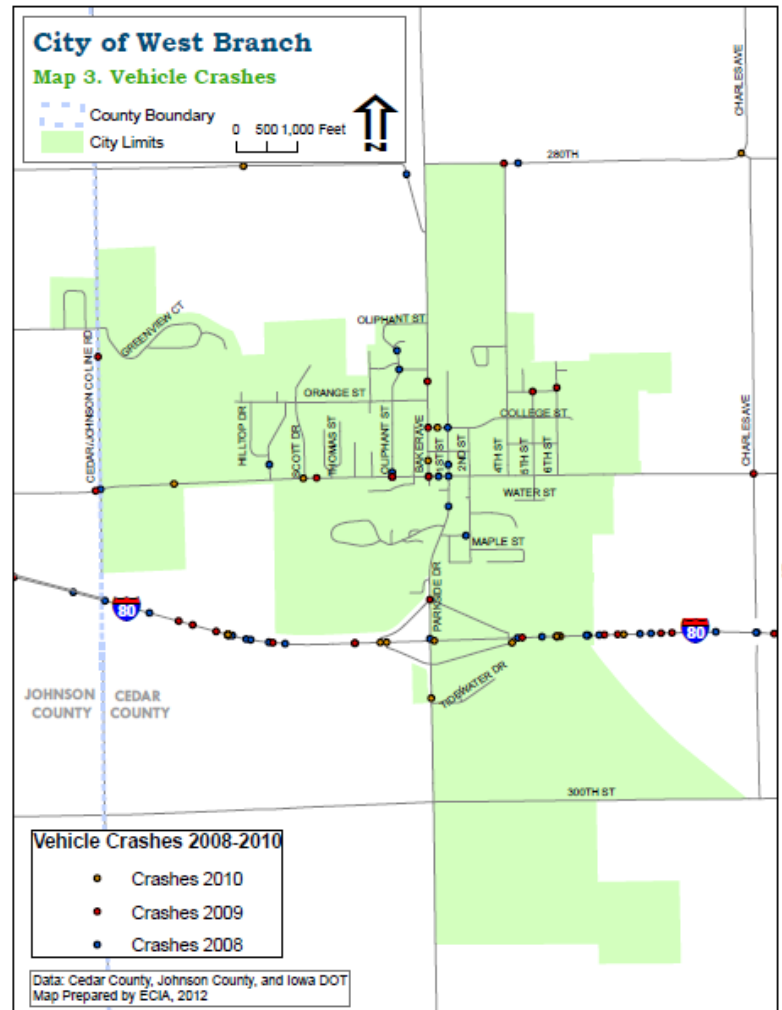
Source: City of West Branch.

Map 9.5. AADT for West Branch Area Rods.

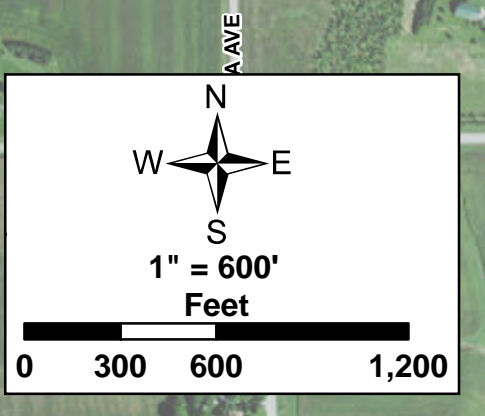


Source: City of West Branch.

Map 9.6. IA DOT Crash Data.



Source: City of West Branch.



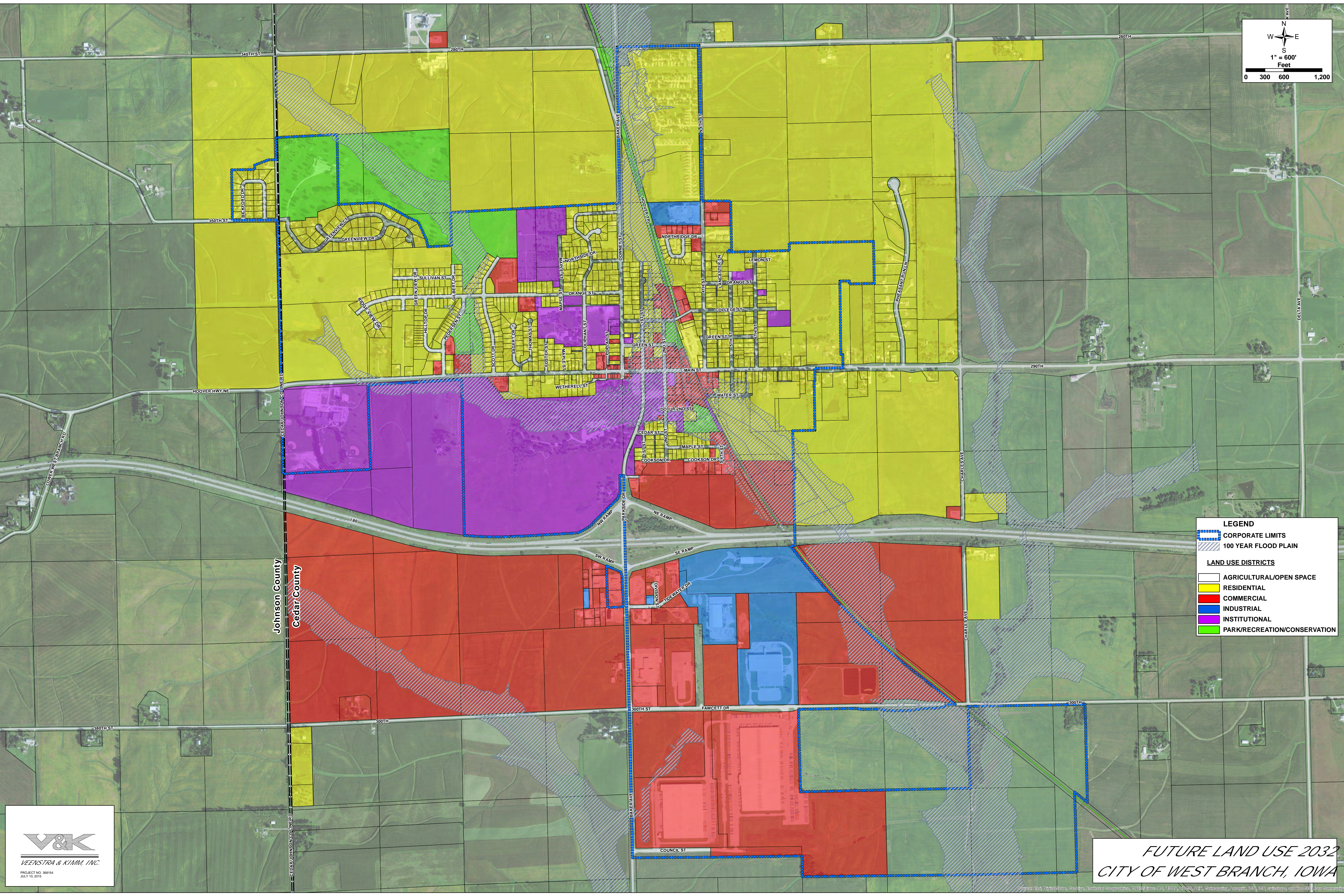
LEGEND

CORPORATE LIMITS
 100 YEAR FLOOD PLAIN

LAND USE DISTRICTS

AGRICULTURAL/OPEN SPACE
 RESIDENTIAL
 COMMERCIAL
 INDUSTRIAL
 INSTITUTIONAL
 PARK/RECREATION/CONSERVATION

FUTURE LAND USE 2032
CITY OF WEST BRANCH, IOWA



VEENSTRA & KIMM, INC.
PROJECT NO. 368154
JULY 10, 2015

Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, IGC, and the GIS User Community